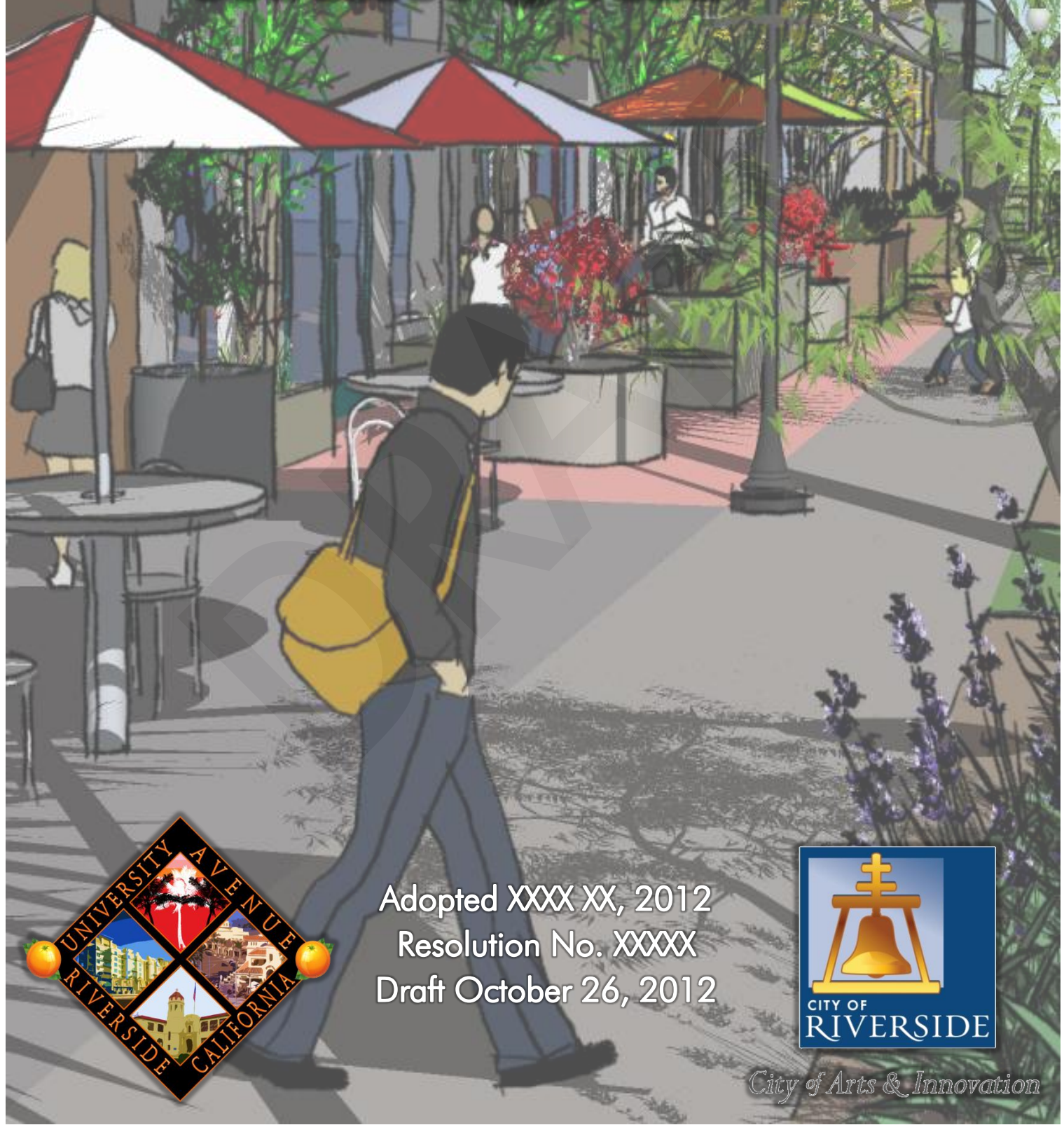
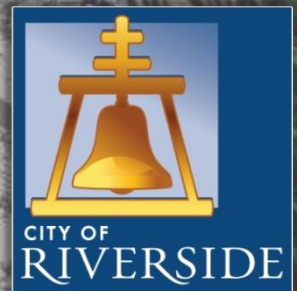


University Avenue

SPECIFIC PLAN



Adopted XXXX XX, 2012
Resolution No. XXXX
Draft October 26, 2012



City of Arts & Innovation

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Acknowledgements

Thank you to everyone who made the University Avenue Specific Plan a success. The City wishes to give special acknowledgement to the contributions of individuals noted below, as well as countless others who provided support.

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Many thanks to Ken Gutierrez, AICP, Retired Planning Director

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SECTION I: CONTEXT





1

Chapter 1: Introduction

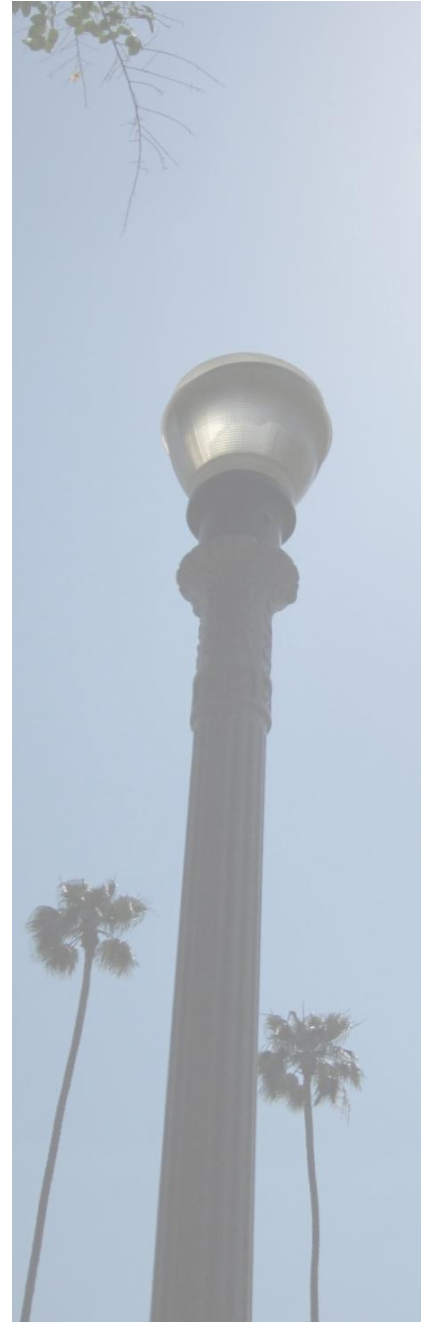
1.1 What is a Specific Plan

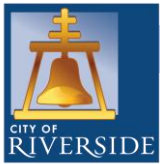
A specific plan is a regulatory tool used by local governments to implement a general plan and guide development in a localized area. While a general plan is the overall guide to manage growth throughout the City, a specific plan is able to focus on the unique characteristics of a special area, such as the University Avenue corridor, by customizing land use regulations for that area. A specific plan is an important and valuable tool to allow the City to work with the neighborhood and stakeholders to create a vital document that reflects a shared vision and values protecting the surrounding neighborhood. A specific plan also helps to remove barriers and encourage compatible development in a defined area.

1.2 Purpose and Intent of the University Avenue Specific Plan

The University Avenue Specific Plan (UASP) establishes objectives, policies, standards, and guidelines for development within the approximately 203-acre University Plan Specific Plan area. The following statements express the purpose for the University Avenue Specific Plan document. The standards and guidelines provided within the plan are intended to:

- A. Create incentives to encourage complementary development efforts to improve the University Avenue corridor and surrounding area as a set of distinct, yet unified districts as defined in the Specific Plan.
- B. Guide development along University Avenue that will help activate the corridor with a variety of housing, restaurants, retail businesses, employment opportunities, and public facilities, becoming a destination of choice for both immediate residents and people throughout the City.
- C. Promote development along the University Avenue corridor that emphasizes public transportation, bicycling and walkability to improve the connectivity between the Downtown business district, the Riverside Marketplace and the University of California-Riverside (UCR) campus while furthering efforts to reduce vehicle miles traveled (VMT) and greenhouse gases (GhG).
- D. Enhance pedestrian mobility by encouraging development that augments the pedestrian path of travel along the University Avenue corridor to accommodate higher levels of foot traffic and gives priority to pedestrians.





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- E. Implement an integrated urban design approach that places buildings closer to the street, deemphasizes the automobile, and creates attractive and useable outdoor gathering spaces that encourage indoor activities to relocate to the outside while drawing pedestrian traffic on-site.
- F. Encourage the preservation and enhancement of historic resources through adaptive reuse of historic buildings.
- G. Promote green building design, construction, and rehabilitation of structures, as well as long-term operation of the facilities to reduce the overall impact of the built environment.
- H. Strengthen and beautify the connections between the University Avenue Corridor and Downtown, the Marketplace, the surrounding residential neighborhoods and the UCR community.
- I. Strengthen the appeal of the businesses and residential areas within the University Specific Plan area to the local residential communities, the UCR community and the overall Riverside region.

1.3 Reason for the Specific Plan Update

The previous University Avenue Specific Plan was adopted in 1993 to establish land uses, standards and guidelines for development in an effort to revitalize the area while strengthening the link between Downtown, the Marketplace and the UCR campus. Since its adoption, a great deal of progress has been made in realizing much of the vision for the University Avenue Specific Plan. However, over the last twenty years that vision has evolved and changed to reflect current development demands, trends in development near college campuses, and trends toward more compact, walkable, and mixed uses. For example, while the previous Specific Plan emphasized the need to enhance the pedestrian-scaled streetscape, substantial building setbacks were required to allow for considerable landscape improvements. The current vision focuses on placing buildings closer to the street and creating outdoor gathering spaces that serve as transition points between the public right-of-way and the various uses along the corridor. The previous University Avenue Specific Plan also fell short in identifying standards and incentives to promote more environmentally friendly development that has become an increasingly important component of regulating documents.

As such, the Planning Division set out to prepare an update to the University Avenue Specific Plan. To assist with the completion of the University Avenue Specific Plan, as well as the Marketplace Specific Plan, the Planning Division submitted a proposal as part of a larger Citywide application for an Energy Efficiency and Conservation Block Grant (EECBG) funded by the American Recovery and Reinvestment Act of 2009 (ARRA). The ARRA was passed by Congress on February 17, 2009 at the urging of President Barack Obama, who signed it into law, as a direct response to the national economic crisis. A grant was awarded to the City of Riverside in late 2009, with \$258,070 allocated to complete both

1: INTRODUCTION



Specific Plans. Work began on the University Avenue Specific Plan October 2009.

Updating the University Avenue Specific Plan under the ARRA grant afforded the Planning Division the opportunity to integrate design guidelines and incorporate elements of the Green Action Plan/Emerald City Plan to promote job creation, improve local air quality, and create an energy efficient and sustainable environment that reduces vehicle miles traveled and greenhouse gas emissions. The updated University Avenue Specific Plan strives to increase the population and employment base, resulting in overall direct economic growth. The Plan also incorporates standards that provide incentives for Green Building/LEED equivalent building and development design. Energy savings for new projects will be measured for all new projects implemented by this Plan. Lastly, the Plan improves bicycle and pedestrian connectivity to Downtown and the Riverside Marketplace and promotes SmartGrowth principles of the General Plan 2025, while reducing VMT and GhG emissions.

1.4 Regional and Local Area Context

Encompassing approximately 81.5 square miles, the City of Riverside is located in the heart of inland Southern California (Figure 1 – Regional Context). The University Avenue Specific Plan area is approximately 203 acres in size and is located in the north central portion of the City, just east of Downtown and the Riverside Marketplace and west of the University of California, Riverside (UCR), within the Eastside and University Neighborhoods. The interchange of State Route 91 (Riverside Freeway) with State Route 60/Interstate 215 is located approximately one mile to the north (Figure 2 – Area Context).

1.5 Specific Plan Area

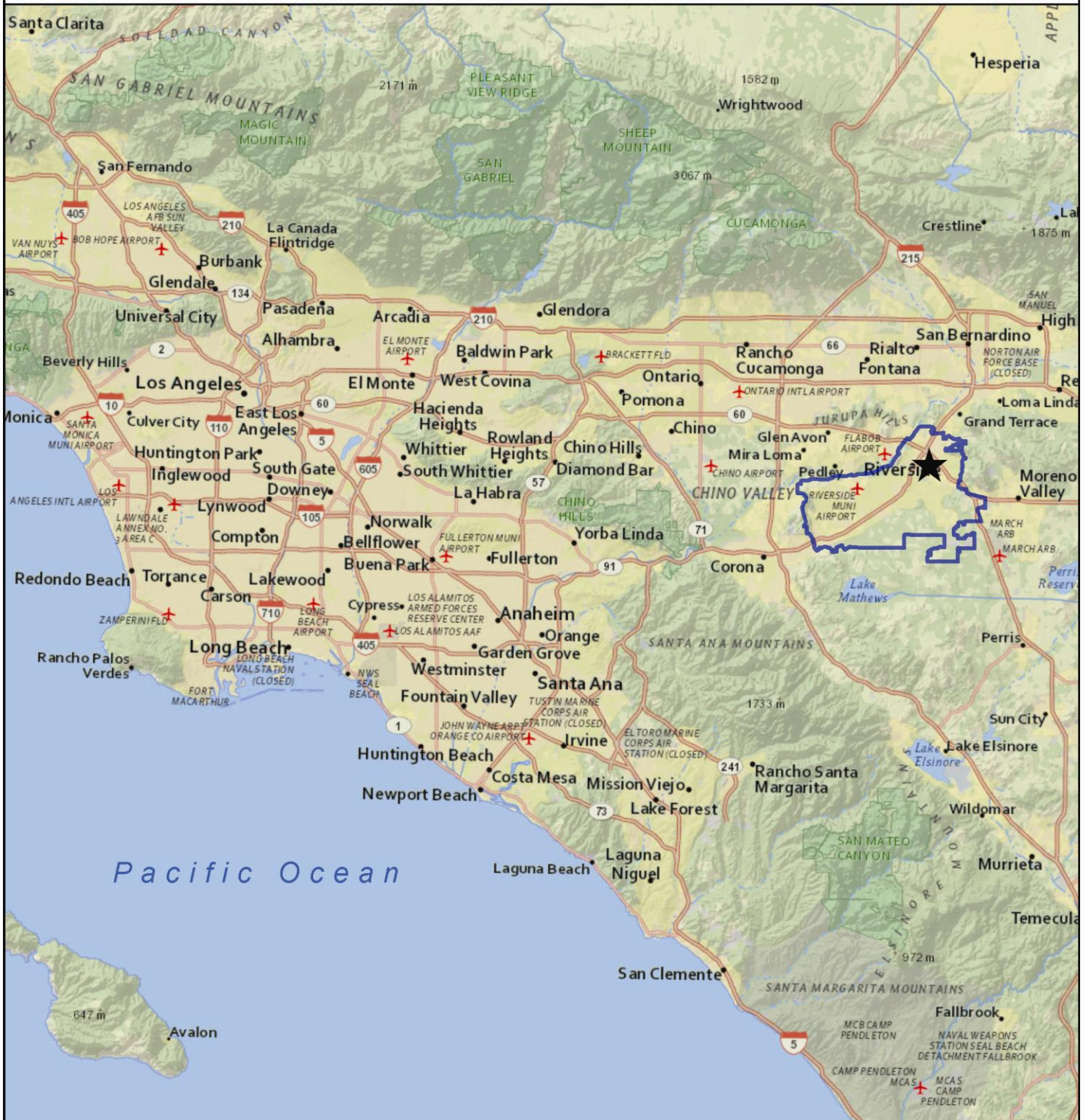
The University Avenue Specific Plan area is bounded by Park Avenue to the west and State Route 60/Interstate 215 to the east. The northern and southern boundaries of the Specific Plan follow the rear parcel lines of all properties that front on University Avenue as well as various commercial and multi-family residential properties that do not front on University Avenue but are nonetheless very much integrated with, and contribute to, the activities along the University Avenue corridor (Figure 3 – University Avenue Specific Plan Boundaries). Further, the following land uses surround the Specific Plan area:

- North: Single and multi-family residential west of Chicago Avenue, manufacturing/warehousing uses and North High School between Chicago Avenue and Iowa Avenue, and Multi-family residential east of Iowa Avenue.
- East: The UCR campus, including a mix of student and family housing, academic and recreational uses.



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Figure 1
Regional Context



City of Riverside



University Avenue Specific Plan

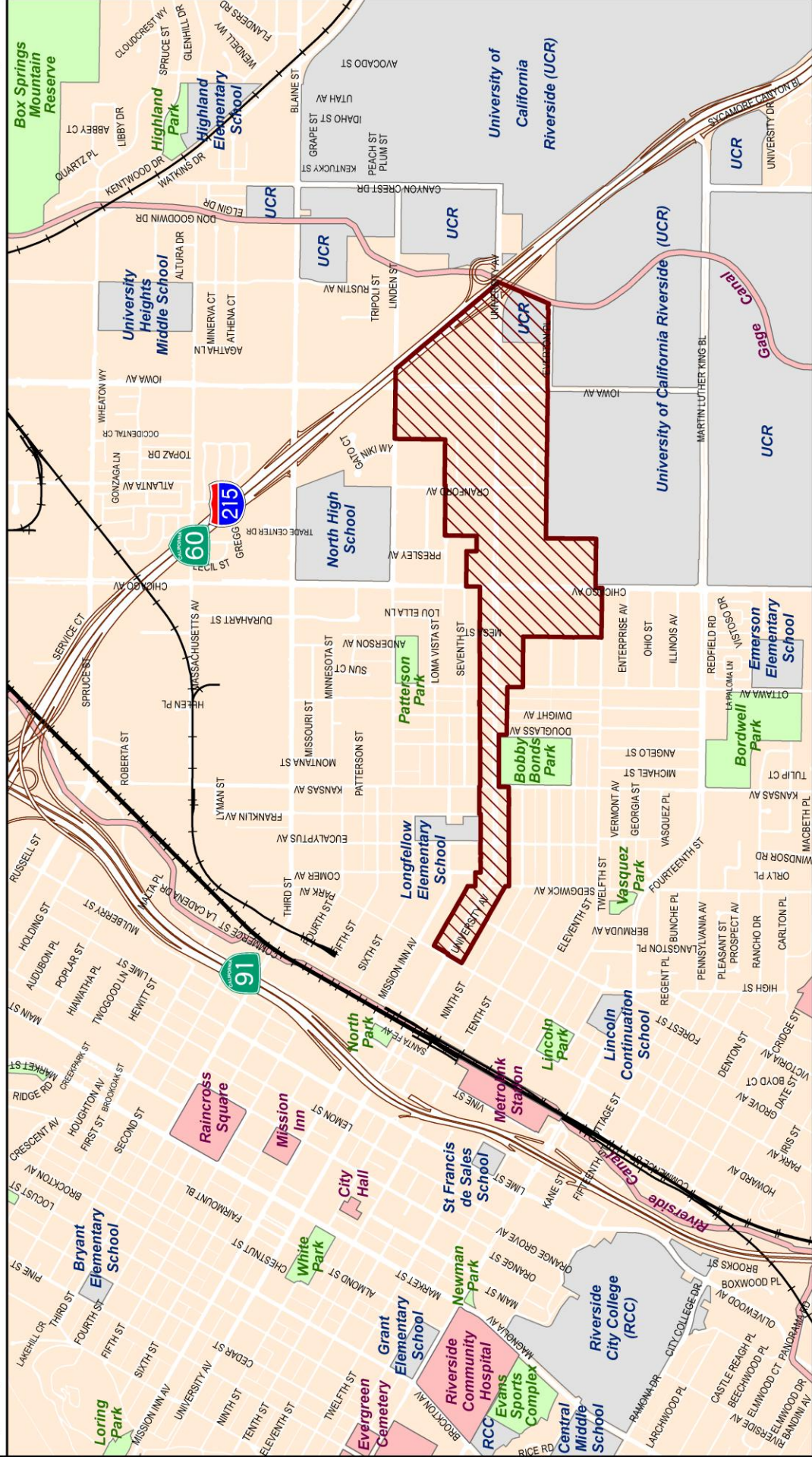


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Figure 2
Area Context

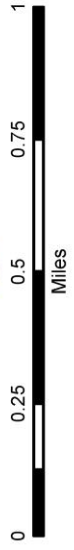


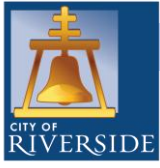
City Features

Parks

Schools

University Avenue Specific Plan





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- South: Single and multi-family residential west of Chicago Avenue and agricultural uses east of Chicago Avenue, owned and operated by the University of California. This area is the site of the anticipated UCR School of Medicine.
- West: The Riverside Marketplace, including the Downtown Riverside Metrolink station and a mix of vacant industrial buildings, restaurants, single-family residential, retail and office uses.

The Specific Plan area consists of four unique, yet interrelated, districts (Figure 4 – University Avenue Specific Plan Districts) and are identified as follows:

- Eastside Neighborhood District
- Commercial Core District
- Chicago-Iowa Avenue District
- University Village District

Table 1.1 identifies the acreage of each district as well as the total acreage for the entire University Avenue Specific Plan area.

Table 1.1 - Specific Plan Land Area

District	Acreage
Eastside Neighborhood District	34
Commercial Core District	46
Chicago-Iowa District	79
University Village District	44
Total Specific Plan Acreage	203

1.5 Contents of Specific Plan

The Specific Plan for the University Avenue corridor has been organized to provide a framework that will ensure coordinated, livable and quality mixed-uses and the preservation of significant historic structures. The plan is organized into the following sections:

Chapter 2: Background: This chapter deals with past planning efforts for this area, community participation, and relationship to the City's General Plan 2025 and the Zoning Code.

Chapter 3: Specific Plan Framework: This chapter describes the area's setting, historical context, and existing land uses, as well as the regional and local circulation, parking, public services, and infrastructure within the Specific Plan area.

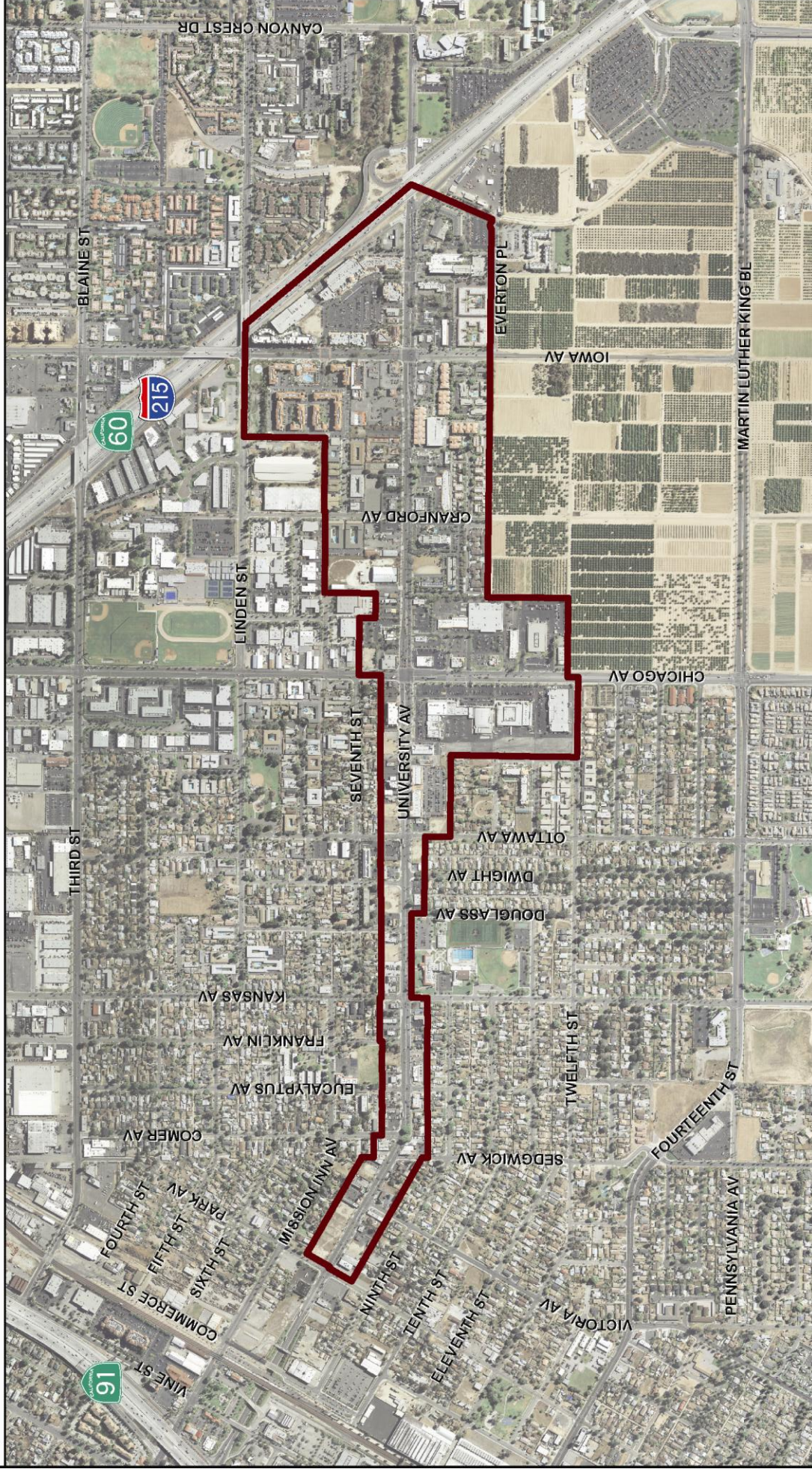
Chapter 4: Visioning University Avenue: This chapter highlights some of the opportunities and constraints within the University Avenue corridor and outlines the vision for the area. A series of Objectives and Policies related to the University Avenue Specific Plan builds upon the vision and is intended to guide the future development of the area.



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Figure 3
University Avenue Specific Plan Boundaries



2012 Aerial Photo

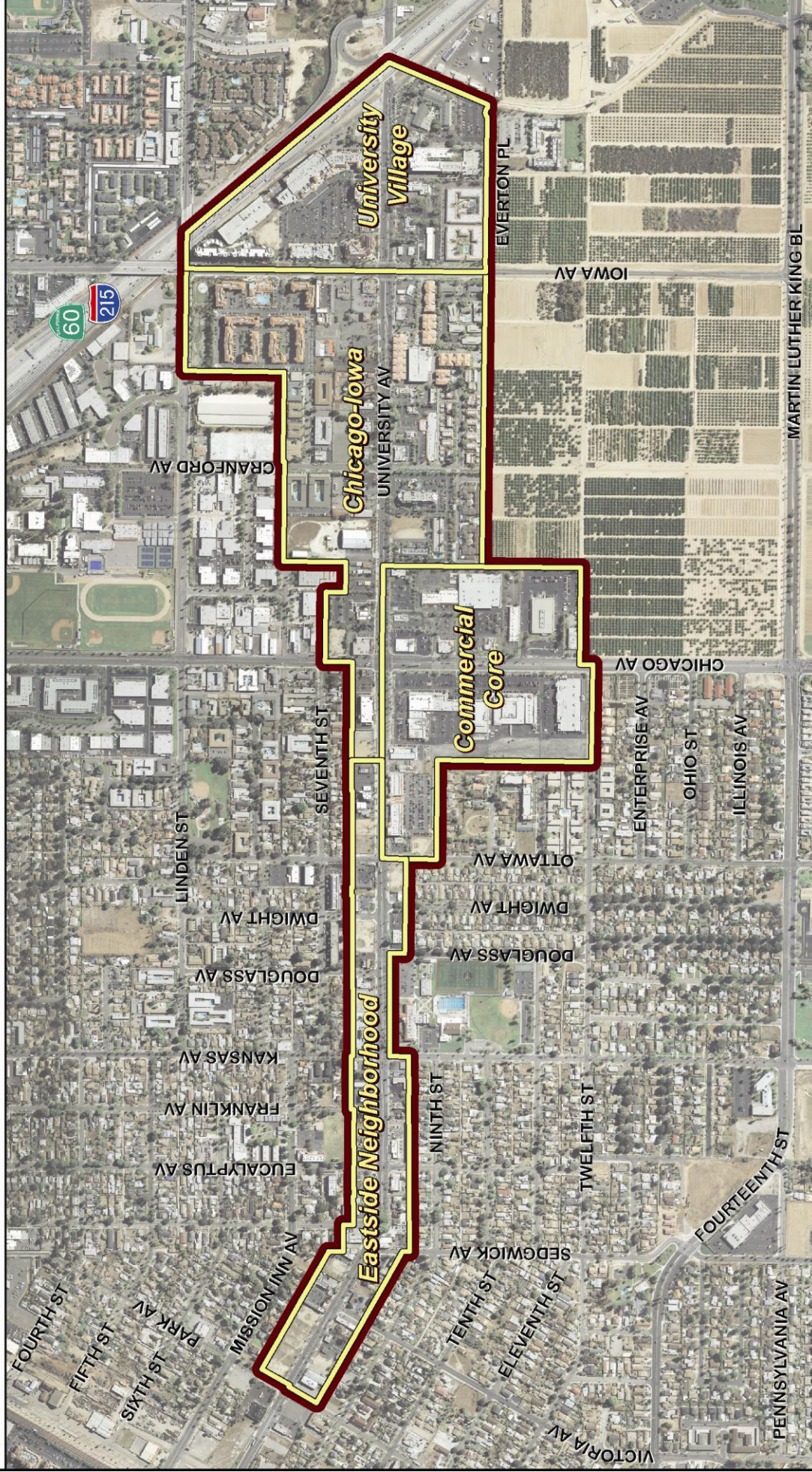


 University Avenue Specific Plan



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Figure 4
University Avenue Specific Plan Districts



2012 Aerial Photo

University Avenue Specific Plan Districts

Specific Plan Boundary



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Chapter 5: Developing Green on University Avenue: This chapter outlines green building incentives and sources for design guidelines for the design, construction, and rehabilitation of structures. In addition, this chapter provides resources to find more information and guidelines for the long-term operation of facilities to reduce the overall impact of the built environment. This chapter strives to promote energy efficiency, improved air quality, water conservation, and reduction in waste and pollution.

Chapter 5 introduces an incentive program called the University Avenue Green Development (UAGD) Program. This program offers valuable benefits for developers that implement a minimum number of green strategies into the design of a project.

Chapter 6: Land Use Regulations, Development Standards and Design Guidelines: This chapter outlines the general development standards that apply to all of the districts as well as general development standards for mixed-use developments in the appropriate districts.

Chapter 7: Streetscape Design: This chapter describes the streetscape design concept for the University Avenue Specific Plan area.

Chapter 8: Implementation: This chapter describes recommended improvements, implementation strategies, sources of financing, maintenance responsibilities and phasing strategy as the means of implementing the Specific Plan. The University Avenue Specific Plan provides for a logical distribution of housing, restaurants, retail businesses and public facilities, serving both the immediate residents and businesses within the project area, as well as being a destination of choice for people throughout the City. The Specific Plan, once adopted, will guide the future development patterns in the University Avenue Specific Plan area.



1.6 General Provisions

Authority and Scope

The adoption of this Specific Plan by the City of Riverside is authorized by Section 65450 et. seq. of the California Government Code. The Government Code authorizes cities to prepare, adopt, and administer specific plans for portions of their jurisdictions, as a means of implementing the General Plan 2025. All specific plans must comply with Sections 65450-65457 of the Government Code. The University Avenue Specific Plan complies with all requirements mandated by State law. The Specific Plan also complies with Chapter 19.820 – Specific Plan/Specific Plan Amendments of the Riverside Municipal Code, all other applicable ordinances of the City of Riverside and shall be adopted by ordinance in accordance with the provisions of the Riverside Municipal Code.



UNIVERSITY AVENUE SPECIFIC PLAN

Application and Conformity

The provisions of this Specific Plan shall apply to all properties included in the Specific Plan area. The boundaries of the approximately 203-acre Specific Plan area are shown on Figure 3.

No construction, modification, addition, placement or installation of any building structure shall occur, nor shall any new use commence on any lot, on or after the effective day of this Specific Plan, except in conformity with the provisions of this Specific Plan.

The provisions of this Specific Plan shall not apply to development projects for which a complete application has been received by the Planning Division as of the effective date of this Specific Plan. However, applicants for such projects may elect to comply with the provisions herein in lieu of the former provisions. Applications for projects whose entitlements and/or permits have expired or were denied are not entitled to the benefit of this section.

Substantial Conformance and Minor Modifications

Minor modifications to the University Avenue Specific Plan shall not require a Specific Plan Amendment. Such modifications shall be subject to a “substantial conformance” determination, an administrative mechanism by which minor modifications to the Specific Plan (which do not result in significant impacts and are consistent with the spirit and intent of the Specific Plan) shall be permitted without a formal amendment process.

In the review of proposals involving the development of existing land, it is recognized that existing site conditions may constrain the extent to which the development standards and guidelines set forth in this Specific Plan can be met. Minor modifications that meet the “substantial conformance” determination may include, but are not limited to, modifications necessary to comply with Final Conditions of Approval or modifications affecting infrastructure, phasing, landscape guidelines, architectural guidelines, lighting guidelines, signage guidelines, and other issues, except those affecting development regulations. Ambiguities and grammatical and spelling mistakes may be clarified as determined appropriate by the City Planner, Community Development Director, or their designee (Approving Authority) without a Specific Plan amendment.

The following minor modifications to the University Avenue Specific Plan shall not require a Specific Plan Amendment, and shall be subject to the “substantial conformance” determination procedure previously set forth:

- Change in utility and/or public service provider or location.
- Change in internal drive alignment, width, or improvements that do not conflict with a standard or condition of approval.
- Minor changes to landscape materials, entry design, and streetscape design which are consistent with the design criteria of the Specific Plan.

1: INTRODUCTION



- Minor changes to the architectural or landscape design guidance or standards, which are intended to be flexible in implementation.
- Modification of any design element in this Specific Plan that improves circulation, reduces grading, improves drainage, or improves infrastructure.
- Changes in project phasing that do not create any new significant environmental impact and are consistent with the intent of the Specific Plan environmental determination.

This list of minor modifications is not inconclusive. Any minor modification that is deemed less than significant by the City Planner, Community Development Director, or their designee (Approving Authority), and in substantial conformance with the purpose and intent of this Specific Plan may be permitted. Any permitted minor modification approved by the Community Development Director shall be filed with the City Clerk by the applicant seeking the minor modification, and any applicable fee to process a minor modification shall be paid upon filing the request for a minor modification. Any modification approved by the Community Development Director shall be kept on file with the Community Development Department. Any requested made by an applicant seeking a minor modification shall be filed with the Community Development Department, and shall include all applicable fees required to process the minor modification.

Interpretation

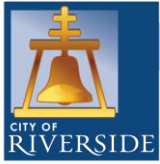
If ambiguity arises concerning the meaning or applicability of any provision of this Specific Plan the City Planner, Community Development Director, or their designee (Approving Authority) shall have the responsibility to review pertinent facts, determine the intent of the provision, and to issue an interpretation as provided for in Chapter 19.060 – Interpretation of Code of the Riverside Municipal Code.

Severance

If any section, sentence, clause, phrase, word, portion, or provision of this Specific Plan is held invalid, unconstitutional, or unenforceable, by any court of competent jurisdiction, such holding shall not affect, impair, or invalidate any other section, sentence, clause, phrase, word, portion, or provision of this Specific Plan that can be given effect without the invalid portion. In adopting this Specific Plan, the City Council affirmatively declares that it would have approved and adopted the Specific Plan even without any portion that may be held invalid or unenforceable.

Land Uses Not Listed

Any use not specifically prohibited in this Specific Plan shall be subject to the Zoning Code (Title 19 of the Municipal Code). In cases where prohibited uses listed in this Specific Plan are inconsistent with the Zoning Code, the Specific Plan shall prevail.



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UNIVERSITY AVENUE SPECIFIC PLAN

Standards Not Listed

Any issue or standard not specifically covered in the Specific Plan shall be subject to the Zoning Code (Title 19 of the Municipal Code). In cases where development standards set forth in this Specific Plan are inconsistent with the Zoning Code, the standards of the Specific Plan shall prevail.

Nonconforming Uses

Any use within the Specific Plan boundary which is nonconforming (legal or illegal) to the requirements and standards of this Plan shall be subject to Chapter 19.080 of the Zoning Code.

Design Review

No new building, structure, outdoor dining area or sign exterior alteration or enlargement of an existing building, structure, outdoor dining area or sign shall be commenced in any district until Design Review approval has been granted pursuant to Chapter 19.710 (Design Review) of the Zoning Code.

Site Plan Review

All new construction and/or rehabilitation of existing construction shall be permitted only as part of a unified development and subject to the approval of a Site Plan Review, in accordance with all requirements of Chapter 6 (General Development Standards) and pursuant to Chapter 19.770 of the Zoning Code.

Treatment of Historic Buildings

Special consideration will be made for any historic property in the University Avenue Specific Plan area. The Secretary of Interior's Standards for Rehabilitation will be applied to specific preservation, rehabilitation, and adaptive reuse projects in a reasonable manner, taking into consideration economic and technical feasibility.

Design Guidelines and Architectural Standards

All properties within the University Avenue Specific Plan shall be subject to the standards within Chapter 6 (Land Use Regulations, Development Standards and Design Guidelines) and Chapter 7 (Streetscape Design) of this document as well as the Citywide Design and Sign Guidelines.

Variances

A variance application may be filed whenever any deviation from the development standard provisions of the Specific Plan is proposed, including, but not limited to, those standards related to height, lot area, yards, setbacks, lot dimensions, signs and parking. Variance procedures shall comply with Section 19.720 (Variance Procedures) of the Zoning Code. Variances to use provisions of the University Avenue Specific Plan are prohibited.



Specific Plan Amendments

A major amendment to the Specific Plan will require review and approval by the City Planner, Community Development Director, or their designee (Approving Authority), the Planning Commission and the City Council. Such major amendments are governed by California Government Code §65500 which requires an application and fee submitted to the Planning Division stating in detail the reason for the proposed amendment.

Appeals

An appeal from any decision, determination, or requirement of staff, the Cultural Heritage Board, or the City Planning Commission shall be made in conformance to the appeal procedures established in Chapter 19.680 of the Zoning Code.

Interpretation of “Should” vs. “Shall”

To assist in understanding the full intent and/or requirement of the various provisions found in the Specific Plan document, users should be informed as to the meaning and context of the words “should” and “shall”, as well as “encouraged” and “discouraged”, as used in the document. These words will be used consistently throughout the document to describe the intent of each objective, policy, standard and guideline.

The use of the word “should” is intended to express the document’s desire and expectation, meant to be applied with some flexibility. It indicates that the document is open to proposals that are equal to, or better than, that stated — as long as the intent is satisfied. The applicant assumes the burden of proof to demonstrate how a proposed project meets this test, and determinations will be made by the City Planner, Community Development Director, or their designee (Approving Authority), per Chapter 19.710 of the Zoning Code (Administrative Design Review).

The use of the word “shall” constitutes a specific requirement by the document. These are absolutely mandatory and offer relatively little flexibility unless choices are provided within the statement itself. All proposals must include these elements as described. Regardless of which term is used, each objective, policy, standard and guideline as it pertains to each individual proposal must be addressed by an applicant.

The use of the words “encouraged” or “discouraged” are intended to express a more or less desirable solution. While, they are not direct requirements, these allow for considerable flexibility and interpretation whose intent must be upheld. Applicants will be expected to prove how proposals implement a particular objective, policy, standard and guideline as deemed applicable by City staff.



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UNIVERSITY AVENUE SPECIFIC PLAN

1.7 Specific Plan User's Guide

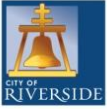
The University Avenue Specific Plan is designed to meet the needs of many users, including property owners, merchants, architects, designers, building contractors, City staff, residents, investors, developers and other interested organizations and persons in the community. Each of these interests plays a vital role in the future development of the Specific Plan Area.

To most effectively use the Specific Plan, the following process is recommended (Figure 5 – User's Guide):

- **Step 1:** Review Chapters 4 to gain an understanding of the overall vision for the Specific Plan.
- **Step 2:** Review Chapter 3 and 7 to gain an understanding of the specific plan framework and infrastructure improvements required to serve the buildout of the Specific Plan.
- **Step 3:** Property owners and/or developers that are interested in developing a specific property within the Specific Plan area should review Chapter 5 to learn about the available development incentives for employing sustainable practices in the construction and operation of projects and Chapter 6 to gain an understanding of the development regulations for their property.
- **Step 4:** Those interested in starting a new business or expanding/enhancing an existing business in the Specific Plan area should review Chapter 6 to gain an understanding of the types of land uses that are allowed and conditionally permitted in the Specific Plan Area.
- **Step 5:** Schedule a meeting with the City of Riverside Planning Division to review your concepts, determine project processes and requirements, identify other agencies that may need to be contacted, and discuss potential issues, solutions and approaches.

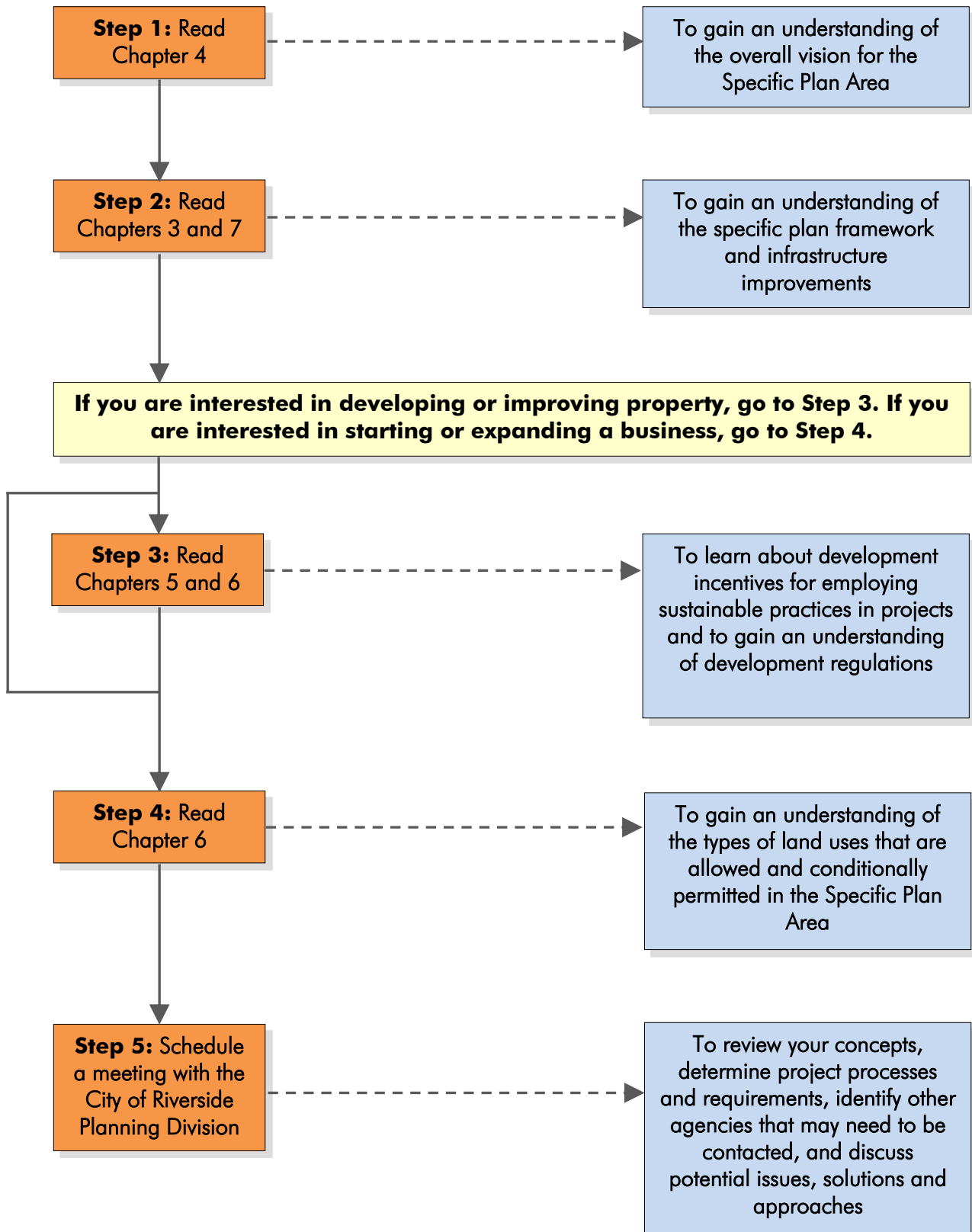
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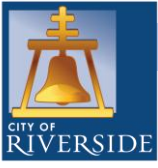
It is highly recommended that interested property/business owners, developers, and investors contact the City of Riverside Planning Division to discuss project proposals before committing resources for plans and drawings.



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Figure 5 User's Guide





UNIVERSITY AVENUE SPECIFIC PLAN

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2

Chapter 2: Background

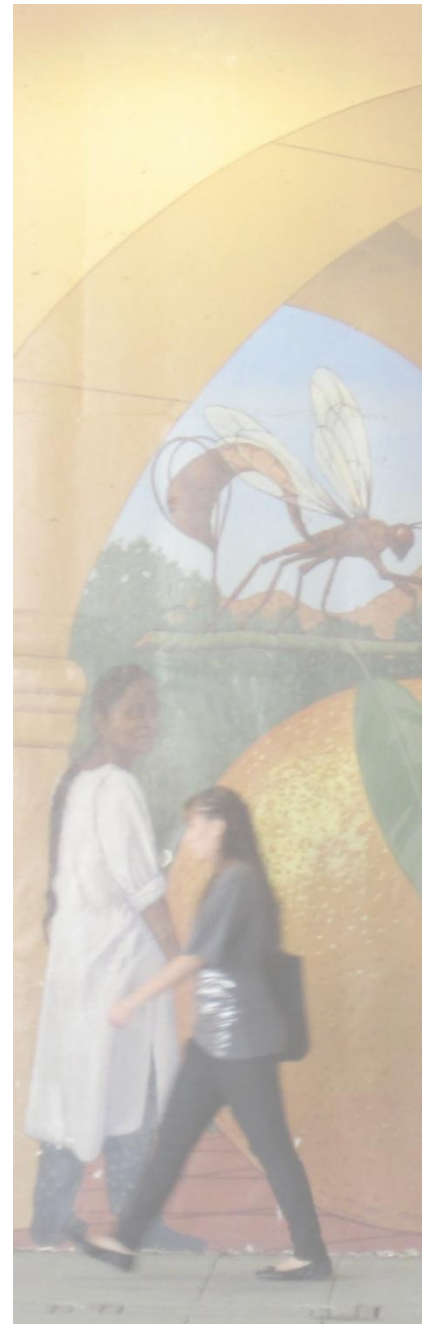
2.1 Related Plans and Studies

In 1993, the University Avenue Specific Plan was adopted to promote revitalization of the University Avenue corridor and improve the physical linkage between Downtown, the Marketplace, and the University of California-Riverside (UCR) campus. Representatives from City staff, the development industry, and a Citizen's Advisory Committee worked to create a development concept for the Specific Plan to:

- Preserve University Avenue as the primary link between Downtown and the UCR campus and maintain the avenue as a four-lane arterial.
- Provide pedestrian amenities and consistent design quality.
- Utilize the existing mature street trees as prime building blocks to enhance the aesthetics and usability of the corridor.
- Implement a multi-modal circulation system that serves local vehicular traffic, public transit, bicyclists and pedestrians.
- Restrict the over-concentration of problem land uses including, but not limited to, motels and liquor stores.
- Facilitate the development of vacant land and underutilized properties.
- Improve the quality of signage along the corridor.
- Encourage lot consolidation to allow for cohesive development.
- Focus on safety and security through environmental design.
- Establish and maintain an appropriate, complementary and viable mix of land uses along the corridor.
- Accommodate the anticipated growth of the UCR community, particularly within the easterly portions of the Specific Plan area.

Many projects have been facilitated by the original University Avenue Specific Plan, including:

- The completion of several phases of streetscape improvements, including historically inspired street lights, street trees, bus shelters, bicycle lanes and landscaped medians.
- The removal of various severely blighted properties.
- The removal of various undesirable uses, including motels and liquor stores.
- The consolidation of lots and creation of numerous opportunity sites for infill development.
- The addition of several public facilities and improvements to Bobby Bonds Park and Sports Complex, including an artificial turf





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football/soccer field, basketball courts, skate park, security cameras and improved lighting.

- The creation of a number of community programs at the Cesar Chavez Community Center, including the Youth Opportunity Center.
- The opening of the Eastside Library and Cybrary and façade improvements at the Chicago Avenue Town Square.
- The implementation of façade and landscape improvements for several properties along the corridor.
- The construction of a number of new commercial, office, residential and mixed use projects, including but not limited to:
 - University Village
 - Grand Marc at University Village
 - University Village Towers
 - The Palms on University
 - Walgreens Pharmacy
 - UEI College
 - McDonald's Restaurant (Gold Certified LEED Building)

In addition to the Specific Plan, there has been a great deal of attention paid to the University Avenue corridor in recent years. There are a number of City plans and studies that influence future development and activity within the University Avenue Specific Plan area, including the documents that are described below.

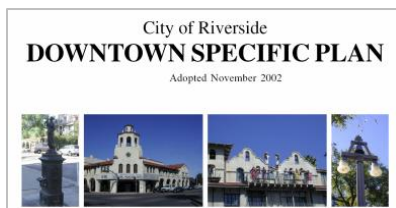


Marketplace Specific Plan

The Riverside Marketplace has particularly important linkages and historic connections to the University Avenue corridor. The Marketplace Specific Plan area consists of 200 acres and adjoins the University Avenue Specific Plan area at Park Avenue to the west. The specific plan was adopted in 1991 to redevelop the area surrounding the Downtown Metrolink Station while preserving its significant historic resources and is an integral component in improving the physical linkage between Downtown, the Marketplace, and the UCR campus. An update to the Marketplace Avenue Specific Plan is currently underway and anticipated to be adopted in 2013.

Downtown Specific Plan

Like the Marketplace, Downtown also has important linkages and historic connections to the University Avenue corridor. Downtown Riverside consists of 640 acres across the Riverside Freeway to the west of the Marketplace. The Downtown Specific Plan was adopted in 2002 and not only establishes development standards for the specific plan area but also sets the tone for the design of buildings within the context of the subarea in which the project may be located to preserve the historic fabric of the Downtown. The Specific Plan also includes several provisions for strengthening the relationship between the three areas, including encouraging new land uses that are complementary to the type of development in the Marketplace and along University Avenue while



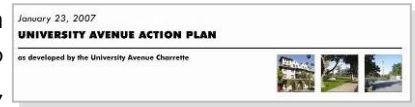
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enhancing the physical linkages that connect the planning areas. Similarly, the update of this plan is intended to ensure that the land use regulations and goals for the University Avenue corridor compliment the Marketplace and Downtown.

University Avenue Charrette and Action Plan

Because University Avenue provides an important link between Downtown and UCR, as well as between UCR and the Metrolink station located within the Riverside Marketplace, the City of Riverside partnered with UCR to develop University Avenue Charrette and Action Plan. Adopted in 2007, this plan analyzed and prioritized key action items that could be accomplished on University Avenue that would make a significant difference in the Avenue's economic viability and quality of life.

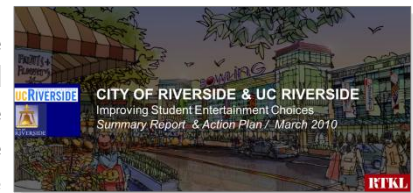


The Action Plan was guided by recommendations from a team of expert consultants from various disciplines and presented before key stakeholder groups representing the business, education, and development communities as well as local residents. These stakeholders helped to further refine the action items.

These action items address a range of issues including safety, security, housing, business attraction and retention, employment, enhanced community facilities, development standards, education opportunities, greater coordination between the City and UCR, and enhanced transit opportunities.

Student Entertainment Charrette and Action Plan

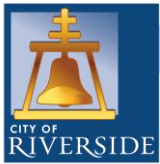
Adopted in 2010, the Student Entertainment Charrette and Action Plan constitute a joint planning effort between the City and UCR to prepare an overall strategy for enhancing student entertainment offerings near the UCR campus. Specifically, the Action Plan identifies opportunities and constraints and proposes a series of recommendations related to land use and design improvements, with a focus on the University Village site. The Action Plan also includes a series of strategy initiatives to guide implementation of the plan.



Eastside Neighborhood Plan

Roughly one-third of the University Avenue Specific Plan area lies within the Eastside Neighborhood, a diverse and vibrant community primarily composed of single-family residential uses with some areas of multiple-family residential uses as well as a concentration of commercial uses located along University and Chicago Avenues. The Eastside Neighborhood has been a part of the City of Riverside since its foundation in 1870, with the early development related to the provision of worker housing to support Riverside's burgeoning citrus industry. While the relative importance of the citrus industry declined over the years, the Eastside remained home to much of the City's workforce for some time.





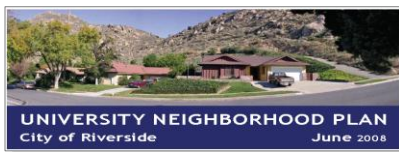
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University Avenue has historically been the main corridor running through the heart of the Eastside, providing a link between Downtown and the UCR campus. The General Plan 2025 envisions mixed-use development along the University Avenue corridor. Further, the Eastside Neighborhood Plan, adopted in 2009, was created to provide a blueprint to enhance and improve the quality of life in the Eastside Neighborhood.

The Eastside Neighborhood Plan was adopted in 2009, and like the University Neighborhood Plan, includes neighborhood-specific objectives and policies as developed with extensive community participation. Roughly half of the University Avenue Specific Plan area falls within the boundaries of the Eastside Neighborhood Plan. Key objectives of the Plan point to the community's desire to preserve and enhance the single-family areas while allowing for a mix of uses including transit-oriented development and enhanced commercial opportunities. Residents also expressed a desire for enhanced parks and recreation opportunities and expanded education opportunities.

University Neighborhood Plan



Rough two-thirds of the University Avenue Specific Plan area lies within the University Neighborhood. The University Neighborhood is a relatively new neighborhood whose development coincides with the growth of the University of California, Riverside (UCR). The northeast portion of the Neighborhood consists primarily of single-family residential subdivisions that were constructed in the 1960s. The remainder of the Neighborhood is comprised of a mix of apartments, manufactured housing developments and single-family residences developed in a more piecemeal fashion over the same time period.

Since its beginnings, the growth and development of the University has been intertwined with the surrounding residential areas. The anticipated growth of the University will continue to impact the surrounding Neighborhood. The Neighborhood's response to growth has created a "town-gown" conflict, where the goals and actions of the University and those of the residents are misaligned. The University Neighborhood Plan, adopted in 2008, is intended to improve the quality of life in the University Neighborhood. The Plan is based upon Objectives and Policies developed as part of the Riverside General Plan 2025, input from residents and property owners, and input from the UCR representatives and research regarding relevant "best practices" in relation to the interface between universities and adjacent neighborhoods.

UCR Long Range Development Plan



The University Avenue corridor is the primary link between the University of California at Riverside (UCR) and Downtown. This linkage is one of both physical appearance and land use. The physical streetscape linkage is an important part of the experience of the UCR community as well as the surrounding neighborhoods. The land use linkage is important both in

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terms of providing residential uses to create a more active Downtown and in terms of providing needed housing supply and living choices for the UCR community.

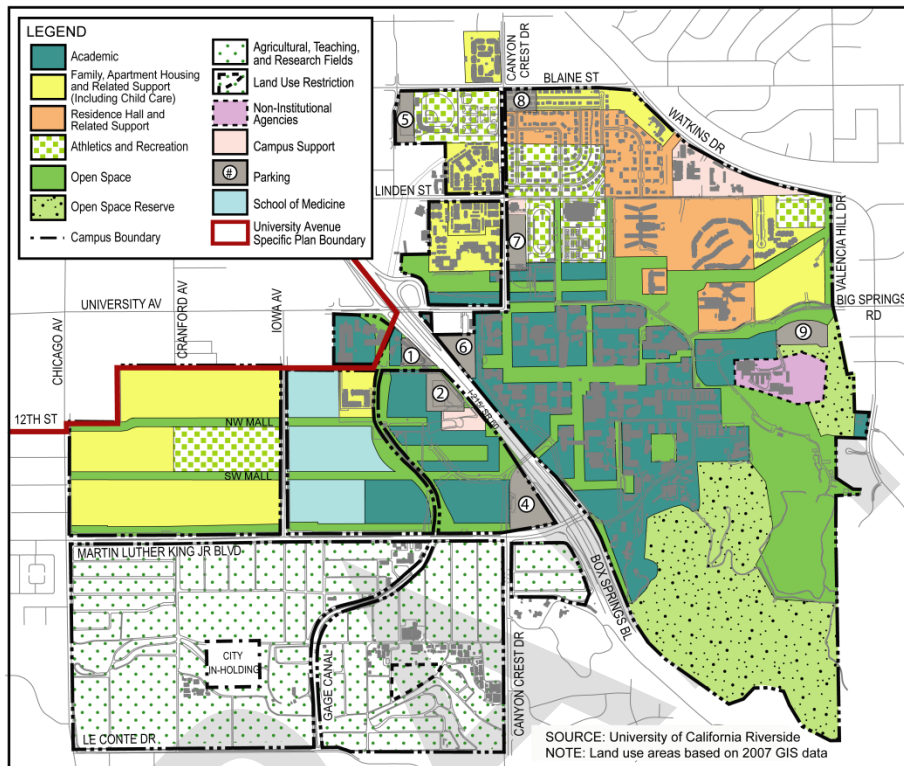
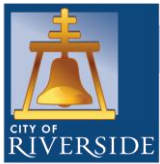


Figure 6: UCR Long Range Development Plan

In 2005 the Regents of the University of California adopted the UCR Long Range Development Plan (LRDP), a plan that guides the development and growth of the UCR campus through the 2020/21 academic year. The LRDP provides for the growth of the campus up to an enrollment level of 25,000 students and 10,540 associated faculty, staff and visitors for a total campus population of 35,540. The 2005 LRDP also provides for the growth in development of up to 14.9 million square feet of student housing, new academic buildings and support facilities including the new School of Medicine (SOM), which is proposed to be constructed on the west campus and located at the northeast corner of Chicago Avenue and Martin Luther King Boulevard. This site adjoins the southeasterly boundary of the University Avenue Specific Plan and encompasses approximately 37 acres mainly used for citrus research (Figure 6).

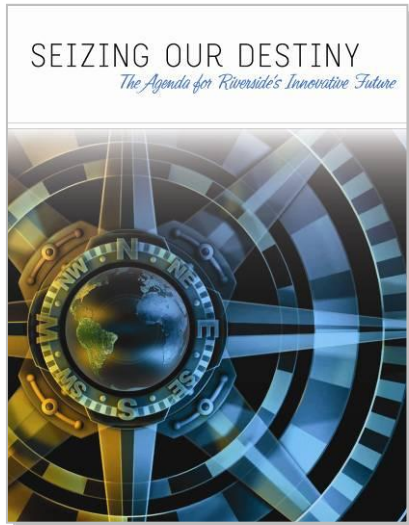


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Seizing Our Destiny: The Agenda for Riverside's Innovative Future

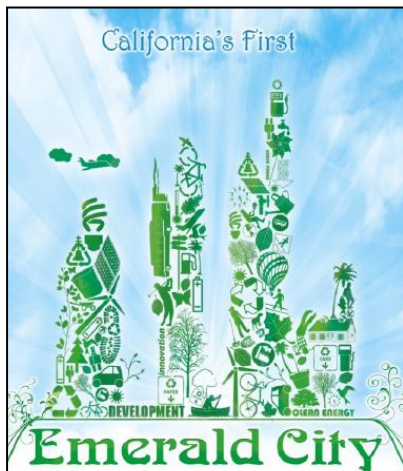
Adopted on May 26, 2010, a document entitled "Seizing Our Destiny: The Agenda for Riverside's Innovative Future" showcases the community's vision for the future of Riverside and highlights the next steps toward this goal. Under the guidance of a steering committee, over 460 Riverside residents provided data that resulted in a bold and ambitious economic strategic vision that centers on the City's economic future through strategic choices for the quality of life of residents. Within Seizing Our Destiny there are a number of Initiatives that may be incorporated or partially implemented within the University Avenue Specific Plan area, including the following:



- 1.4 Streamline government procedures, processes and forms
- 1.5 Create incentives that ensure the highest levels of business success
- 4.3 Integrate other educational and research programs with the UCR Medical School
- 5.2 Create a "Green Business District"
- 6.1 Establish five highly walkable destinations
- 6.2 Provide exciting, edgy, fun and safe arts and cultural opportunities and venues for young people
- 9.2 Develop performance and exhibition spaces

Green Action Plan/Emerald City Plan

The City's Green Action Plan is based on guidelines established by a Clean & Green Task Force appointed by Mayor Ronald O. Loveridge and aimed at solidifying Riverside as a leader in clean and green practices. The Green Action Plan/Emerald City Plan outlines many goals and strategies in eight categories: energy; greenhouse gas emissions; waste; urban design; urban nature; transportation; water; and healthy communities. Due in part to the adoption of this Plan, the City of Riverside was the first California city to be designated as an Emerald City by the California Department of Conservation.



Goal 9 of the Green Action Plan seeks to use specific plans along the Bus Rapid Transit (BRT) corridors, such as the University Avenue Specific Plan, to address infrastructure systems, revitalize urban and community centers, and promote infill and compact development. The Green Action Plan specifically calls for amending the University Avenue Specific Plan to create incentives for high density and mixed-use opportunities along the BRT corridor that include greater densities for greener design. This update to the University Avenue Specific Plan provides the perfect opportunity to implement the Green Action Plan/Emerald City Plan. Beyond the inclusion of objectives and policies reflecting the Green Action Plan/Emerald City Plan, the University Avenue Specific Plan directly encourages green development through the University Avenue Green Development (UAGD) program, which is designed to encourage sustainable practices in the

2: BACKGROUND



construction and operation of projects within the Specific Plan area (see Chapter 5 for a complete description of the UAGD program).

City of Riverside Economic Development Action Plan

Adopted by action of the City Council on February 28, 2012, the Economic Development Action Plan is an interdepartmental implementation plan that calls for quick and aggressive measures that strive to ensure that the City of Riverside stays poised for economic and community success through job creation, promotion, and retention. Building upon the community vision established in "Seizing Our Destiny" the Plan establishes a series of action steps that are aligned within four aspects of the community vision:

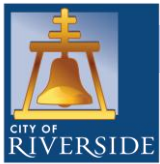
- Intelligent Growth: Economics, People, Sustainability and Transportation;
- Catalyst for Innovation: Discovery, Technology, Education and Connection;
- Location of Choice: Public Health, Recreation, Culture and Neighborhood; and
- Unified City: Giving, Diversity, Engagement and Pride.

It is intended that all of the action items listed within the Economic Development Action Plan be completed in 12 months, with a progress update report due to City Council in 2013. Additional action items may also be established at that time.

2.2 Community Participation

The University Avenue Specific Plan was developed with the participation of residents, businesses, property owners and other stakeholders along the University Avenue corridor, the Eastside and University neighborhoods, and the UCR community. Several opportunities were provided throughout the planning process to allow the various stakeholders to offer substantial feedback in the development of the Specific Plan, as follows:

- Citizens Advisory Committee (CAC): Formed early in the process, the CAC consisted of a small group of residents, business owners and other community stakeholders that consulted with City staff to discuss the vision for the future development of the University Avenue corridor. The main purpose of the CAC was to draft the vision statements and identify opportunities and constraints for each of the four districts within the Specific Plan area to present to the larger community for review and comment.
- Riverside Neighborhood Conference: City staff hosted a booth featuring the University Avenue Specific Plan as well as the Marketplace Specific Plan at the 2012 Neighborhood Conference, held on June 2, 2012 in Downtown Riverside. The purpose of the event was to provide the community at large further opportunities to offer feedback on both of these Specific Plans.

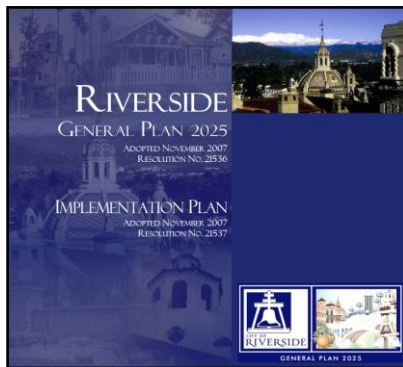


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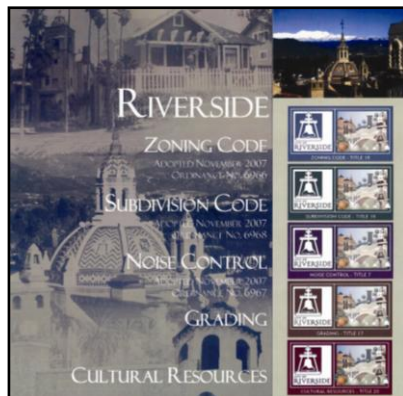
- Planning Commission Workshop: A public workshop was held on November 8, 2012 providing Planning Commissioners and community stakeholders an opportunity to review and provide further recommendations related to the vision of each specific plan district as well as the previously-established objectives, policies, and standards for each district.

2.3 Relationship to the General Plan 2025



To achieve the Vision of University Avenue becoming an active and lively corridor that seamlessly links Downtown, the Marketplace and the UCR campus, the General Plan Land Use designation of the majority of the properties within the Specific Plan area were changed during the General Plan 2025 Program update (adopted December 2007) to Mixed-Use Neighborhood (MU-N), Mixed-Use Village (MU-V), or Mixed-Use Urban (MU-U) (Figure 7 – Existing General Plan). To ensure that the University Avenue Specific Plan remains consistent with the General Plan 2025, the General Plan 2025 Land Use designations are not proposed to be amended with the adoption of this Plan. This is consistent with General Plan 2025 Goal LU-5 and associated policies that provide for the use of “Area Plans, Neighborhood Plans or Specific Plans” as part of the General Plan 2025 to address detailed design, land use and policy direction for a particular area within the City.

2.4 Relationship to the Zoning Ordinance



Adoption of this update to the Specific Plan amends the existing University Avenue Specific Plan Overlay Zone, which incorporates all of the standards for land use and development set forth in this Specific Plan. As such, the current base zone of each individual property within the Specific Plan area dictates the standards for land use and development (Figure 8 – Existing Zoning). The regulations of this Specific Plan are in addition to those set forth in the planning and zoning provisions of the Riverside Zoning Code and any other applicable ordinances. The Specific Plan does not convey any rights not otherwise granted under the provisions and procedures contained in the Zoning Code and other applicable ordinances, except as specifically provided herein.

Where land use regulations and/or development standards of the Zoning Code are inconsistent with this Specific Plan, the standards and regulations of the Specific Plan shall prevail. Any issue not specifically covered in the Specific Plan shall be subject to the Zoning Code and/or Municipal Code. Interpretations may be made by the City Planner, Community Development Director, or their designee (Approving Authority) or referred to the City Planning Commission if not specifically covered in the City's existing regulations.

2: BACKGROUND



Rezoning for Mixed Use

While the current base zone of each individual property within the Specific Plan area dictates the standards for land use and development, it is strongly recommended that properties be rezoned to the corresponding mixed-use zone per the Zoning Code and in accordance with their respective General Plan Land Use designations, as they are developed. To encourage mixed use development on these properties, it is recommended that the City initiate the necessary zone changes at the time that a viable project is submitted to the Planning Division for design review.

General Plan 2025 Housing Element Conformance

As described in Chapter 6, Section 6.7 – Commercial Core District, the recent General Plan 2025 Housing Element update has identified a site within the Commercial Core District as a potential infill site for housing. As such, a city-initiated rezoning to MU-U will be processed for this site in conjunction with this update to the University Avenue Specific Plan, consistent with the General Plan 2025 Housing Element and in accordance with State of California Planning and Zoning Law.

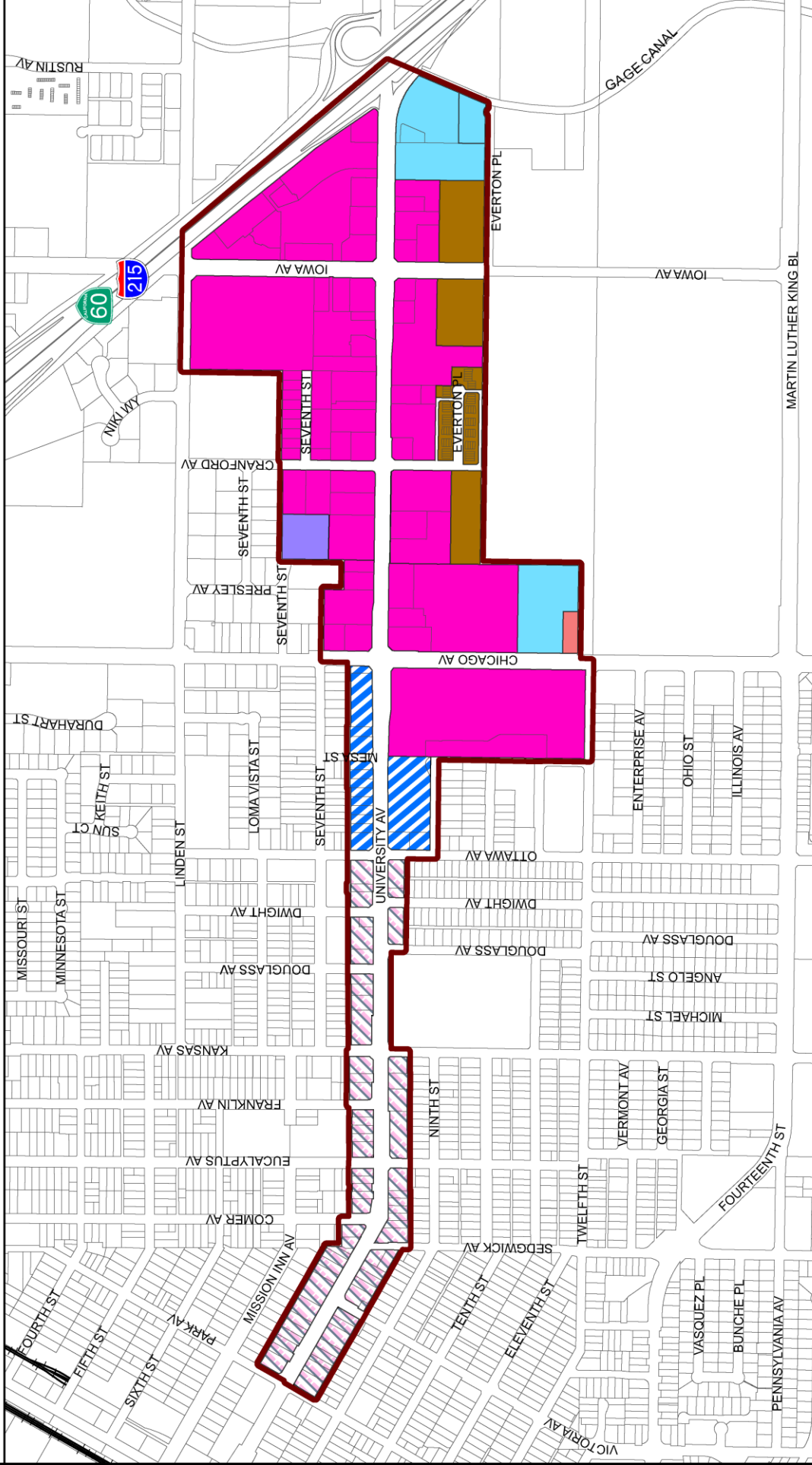
Signage

All signage within the Specific Plan Area shall be subject to the Riverside Zoning Code, Chapter 19.620 – General Sign Provision, as well as the Citywide Design and Sign Guidelines.



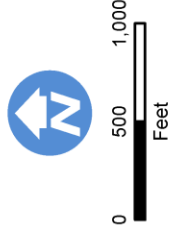
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Figure 7
Existing General Plan



- Business/Office Park (B/OP)
- Commercial (C)
- High Density Residential (HDR)
- Mixed-Use Neighborhood (MU-N)
- Mixed-Use Urban (MU-U)
- Mixed-Use Village (MU-V)
- Public Facilities/Institution (PF)

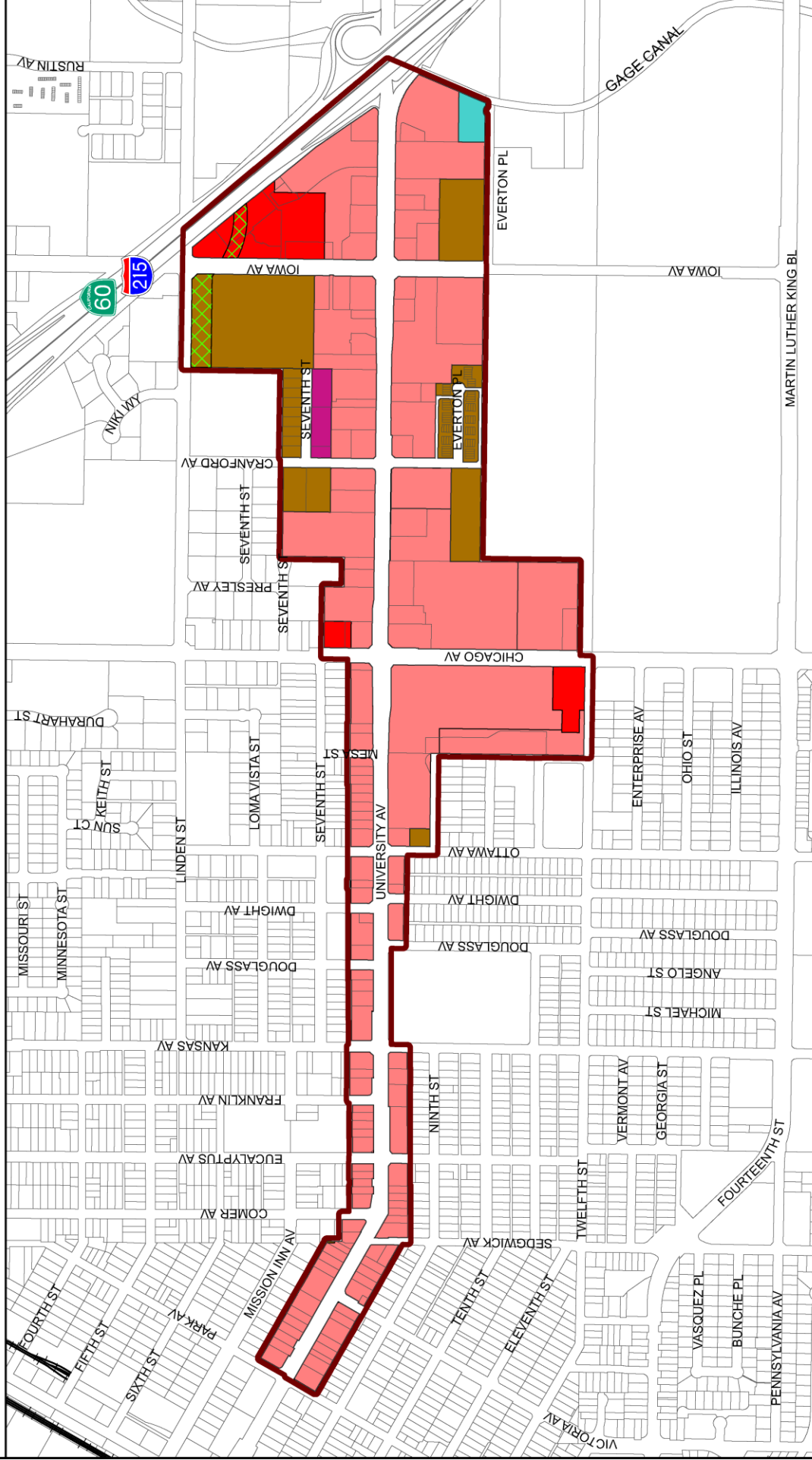
Specific Plan Boundary





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Figure 8
Existing Zoning



Commercial General (CG)

Commercial Retail (CR)

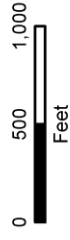
Office (O)

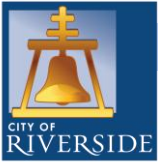
Public Facilities (PF)

Multi-family Residential (R-3-1500)

Water Course Overlay Zone (WC)

Specific Plan Boundary





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UNIVERSITY AVENUE SPECIFIC PLAN





3

Chapter 3: Specific Plan Framework

3.1 Historic Context

Introduction

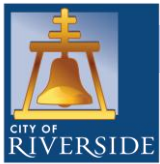
The significance of University Avenue rests in its function historically as a principal east-west thoroughfare through the northern portion of the City of Riverside and partly in its name, which, when changed from Eighth Street in 1966, highlighted the important link to the University of California, Riverside (UCR). Until the mid 1950s, Eighth Street was primarily residential, dominated by single-family homes and punctuated by a scattering of neighborhood shops; its boundary was Chicago Avenue on the east. The City annexed Eighth Street from Chicago Avenue east to the Gage Canal (just east of today's Interstate 215) for commercial and industrial use in 1957. As it became a segment of State Route 60, University Avenue began to cater to automotive audiences who were both tourists and residents, with offerings of motels, gasoline stations, and drive-up restaurants. Offices and branch banks added to the mix by the early 1960s.

Development of the Avenue

The area of the University Avenue Specific Plan encompasses portions of two neighborhoods: Eastside and University. The area of the Specific Plan from Park to Chicago Avenue represents a portion of Eastside Neighborhood; Chicago Avenue east to UCR represents a portion of the University Neighborhood. The portion representing the University Neighborhood was largely citrus and agricultural acreage through at least the 1930s. The Eastside Neighborhood was historically comprised of the first additions to the City as Riverside grew beyond the original Mile Square. The development of most of these tracts marked the boom of the 1880s, which brought prosperity, population growth, and a vision for the future development of Riverside. The boom was initiated by new industrial technologies, new modes of managerial organization that increased efficiencies in agricultural production and distribution, and probably most significantly, the establishment of the Gage Canal. These improvements had a great impact on the early spatial development of Riverside and formalized the industrial and transportation corridor connecting the urban and agricultural areas, along which light industry, worker housing, and related citrus commercial developments came to reside.

The Gage Canal, completed in 1887 by Matthew Gage, brought a reliable source of water for both residential and agricultural uses to the City and greatly enhanced the land values east of the Mile Square and let





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to an eastward expansion of development. Upon completion, the canal irrigated 12,000 acres to the east and south of the original colony, including nearly every lot in what was known as White's Addition, a 100-acre triangular-shaped subdivision located east of what is now Commerce Street between Third and Tenth Streets (Figure 9). White's Addition was purchased from land originally part of John W. North's holdings and is now part of the Eastside Neighborhood and represents a portion of the University Avenue Specific Plan area (then Eighth Street).

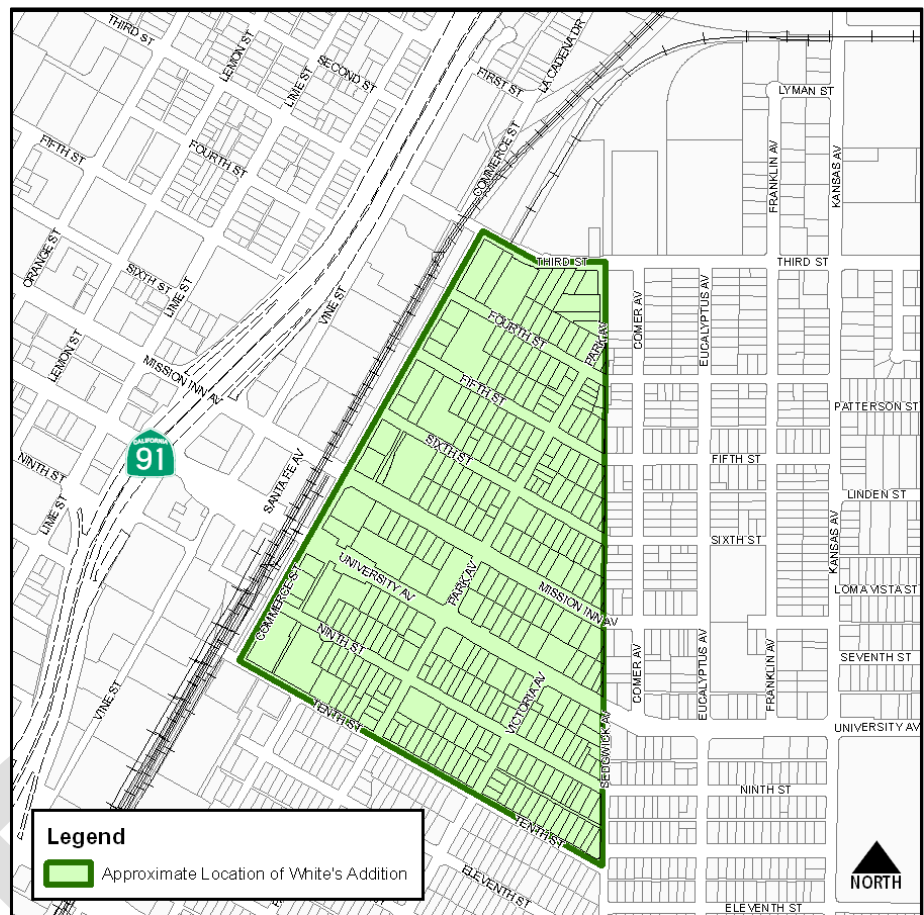


Figure 9: White's Addition of 1887

White's provision of sidewalks and a tree-lined streetscape came at private expense, and for fourteen years a caretaker was even provided to ensure that the trees, which were likely irrigated by the water piped to each lot from the Gage Canal, would survive. He did so well in advance of the City Beautification efforts of municipal reformers who in 1906 campaigned for municipal tree planting and caretaking services, establishing the first such program in the west. Ultimately this led to the planting of over 30,000 trees in the city, including rows of palms and pepper trees that still line the North-South railway strip of Pachappa Avenue (now Commerce Street).

3: SPECIFIC PLAN FRAMEWORK

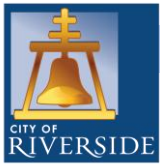


Between 1871 and 1882, Riverside's orange, lemon, and lime groves had become productive enough to put the city at the center of California's citrus industry, close behind Los Angeles. While in 1862 there were only about 25,000 orange trees planted in the state, by 1882 there were half a million, with nearly 250,000 in Riverside alone. This booming industry provided opportunities for enterprising individuals to start small businesses, providing services to the community. By the 1920s there were well established commercial areas on University Avenue (then East Eighth Street) and Park Avenue, serving the local community.

It was around this time that Frank Miller began developing and creating the now iconic Mission Inn in the Downtown area. The Mission Inn was owner Frank Miller's obsession through the 1930s, and he added to it incessantly with arcades, bell towers, red tile roofs, Mexican mosaics, patios, courtyards, a cathedral, and, in the Cloister Wing, a warren of underground galleries called the Catacombs that housed a small portion of his vast art collection. Miller built its identity and that of the city around a polyglot of Mexican, Indian, and Spanish references with a fair number of Moorish and Byzantine details thrown in for good measure, not unlike nearby Southern California Missions. These styles also influenced the packing houses, showrooms, and industrial buildings near Pachappa Avenue (today's Commerce Street) and Vine Street as well as through the growing Eastside and later University Avenue, which would continue contributing to the iconographic identity for Riverside and its material expression.

Also by the 1920s, the tourist gambit also included March Air Force Base, which had opened as Alessandro Field southeast of the city boundaries in the teens and expanded in 1927. It also included the chance to see modern science in action: the Citrus Experiment Station. Partly the result of vigorous lobbying by local constituents interested in statewide efforts to apply industrial and scientific approaches to agriculture, the Agricultural Experiment Station had first been established by the California State Legislature and opened with just two cottages and a stable on the east side of Mt. Rubidoux in 1907. Within ten years, the station had moved to the base of the Box Springs Mountains where it attracted the interest of scientists and agriculturalists, but served only a handful of students. This location thus became the eastern terminus of Eighth Street, which, according to the 1951 Sanborn Fire Insurance Map and City building permits, remained a largely residential corridor with a few commercial uses at the western end until the end of the early 1950s.

During the 1950s, Eighth Street was a segment of State Highway 60 which ran from Mission Boulevard east through Downtown. State Route 60 functioned as an important link between the Los Angeles area and communities to the east and the desert. By the late 1950s, the impact of motorized consumers traveling along State Route 60 began to be reflected in the built environment and this change was reflected in the City's 1956



UNIVERSITY AVENUE SPECIFIC PLAN

Zoning Map where Eighth Street was designated as a General Commercial Zone.

Eighth Street was radically transformed when, in 1954, the University of California (UC) Regents and State Legislature authorized the creation of a Riverside UC campus, which opened with 127 students and 65 faculty members. It grew quickly from there, becoming a general campus serving undergraduates and graduate and professional students in 1959. A year later, the College of Agriculture became part of the campus, to continue the work of the former Citrus Experiment Station. The UC Regents also agreed to the City's annexation of the land including and around the campus

With the 1957 opening of the Riverside International Raceway in what is now Moreno Valley, Eighth Street saw the replacement of many of the single-family residences with the development of restaurants, motels, car washes and service stations to serve the many travelers visiting the area. Also in 1957, Riverside benefited from state and federal funding for limited-access freeway construction, as old Highway 395 was improved and renamed the Escondido Freeway (I-215), which still serves as the major north-south route connecting San Diego, Riverside, and San Bernardino. The Riverside Freeway (SR-91) linking Riverside to the Harbor Freeway (SR-110) opened in 1961, replacing old Highway 18. The Pomona Freeway (SR-60), beginning in East Los Angeles and terminating in Riverside also opened in 1961. As a result, Eighth Street was quickly bypassed by the creation of the new freeway system, leading to the financial decline of many businesses throughout the 1970s and '80s due to the loss of direct contact from motorists.

In 1966, the University of California Board of Regents established a range of 15,000 to 27,500 students for UCR, and sought to expand further still, using citrus land south of the freeway for academic structures and other campus activities. At this time, Eighth Street was changed to University Avenue, reflecting the ways in which the University had intertwined with the neighborhood, the City, and the state.

Historic Resources within the University Avenue Specific Plan Area

As a result of the rich and varied history of University Avenue, a reconnaissance-level survey of every property within the project area was performed and a historic Context Statement was prepared in conjunction with the writing of this Specific Plan. This survey confirmed, reinforced, and occasionally enhanced the results of previous surveys within the area.

Based on the reconnaissance-level survey and basic research, two of the properties appear to meet the criteria for listing in the National Register: 2211 University Avenue (former Fire Station #4 designed by G. Stanley Wilson) and 1393 University Avenue (formerly the Farm House Motel, an example of the 1950s automobile-oriented/roadside architecture). Another 4 properties appear eligible for listing in the California Register of

3: SPECIFIC PLAN FRAMEWORK



Historical Places: 1320 University Avenue (formerly Sandy's, now IHOP); 1665 University Avenue (formerly Arby's); 1855 University Avenue (currently a dentistry office); and 1955 University Avenue (formerly Denny's).

For local designation purposes, 3 properties are either already designated or appear eligible as contributors to a Historic District and 13 properties are either already designated or are eligible for designation as a City Landmark or a Structure of Merit. Each of these properties is subject to the California Environmental Quality Act when development projects are proposed for the site.

Figure 10 illustrates the historic context of the University Avenue Specific Plan area and highlights National Register and National Register Eligible sites, City landmarks, and City Structures of Merit, as well as properties that have been identified as being eligible for state or local historic designation. This figure also highlights some of the key identifiable historic resources within the community.

3.2 Existing Land Uses and Setting

As shown in Figure 11 – Existing Land Uses, each District within the Specific Plan area encompasses a diverse set of land uses. Within the Eastside Neighborhood District, there are also numerous vacant parcels primarily located west of Chicago Avenue. This District features a mix of neighborhood-serving commercial, restaurant, office, and other uses, as well as several long-established motels. Although not within the boundaries of the University Avenue Specific Plan, the centerpiece of the District is the Caesar Chavez Community Center and Bobby Bonds Sports Complex. Overall, the District is comprised of mostly small-scaled, one-story buildings on shallow lots that abut single-family residences or access alleys along the rear of the properties. Due to recent redevelopment efforts, many neglected properties and properties developed with undesirable uses within this District have been acquired, razed and/or consolidated. This has resulted in several vacant opportunity sites ripe for infill development.

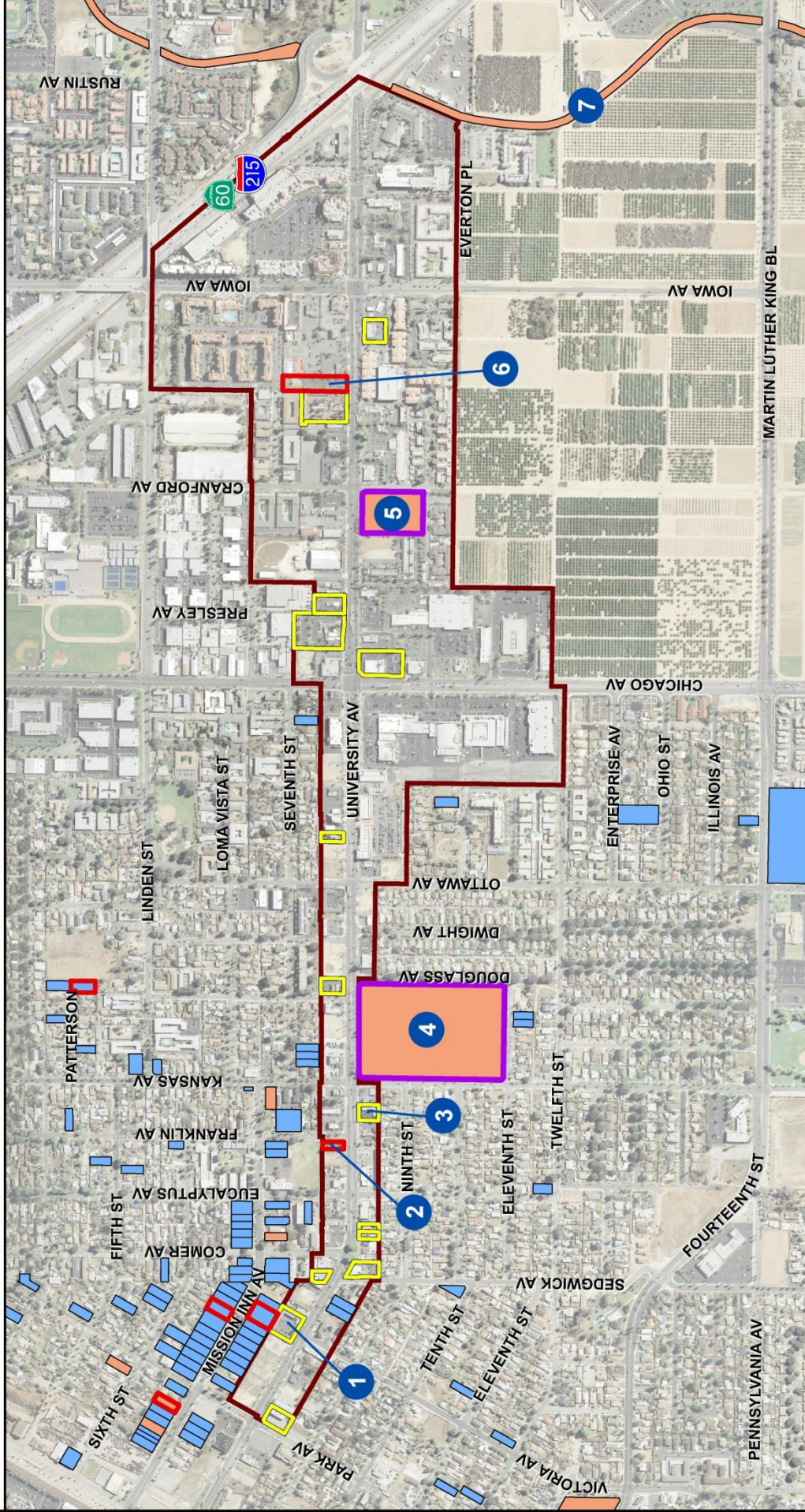
Located in both the Eastside and University Neighborhoods the Commercial Core District is the geographic center of the Specific Plan area and contains many of the broader-appeal uses and amenities that serve the surrounding neighborhoods as well as the UCR community. This District is mostly comprised of two large shopping centers that contain a large mix of commercial retail uses, including pharmacies, apparel stores, restaurants, personal services, grocery stores and banks.



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Figure 10
Historic Framework



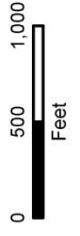
2012 Aerial Photo

Community Identifiable Historic Resources

- 1 Thunderbird Lodge Pylon Sign
- 2 Former Fire Station #4
- 3 Skylark Motel
- 4 Cesar Chavez Community Center/
Bobby Bonds Sports Complex
- 5 Weber House
- 6 Farm House Motel
- 7 Gage Canal

- National Register Sites
- National Register Eligible Sites
- City Landmarks
- City Structures of Merit
- Eligible for Historic Designation

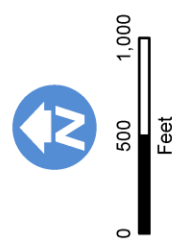
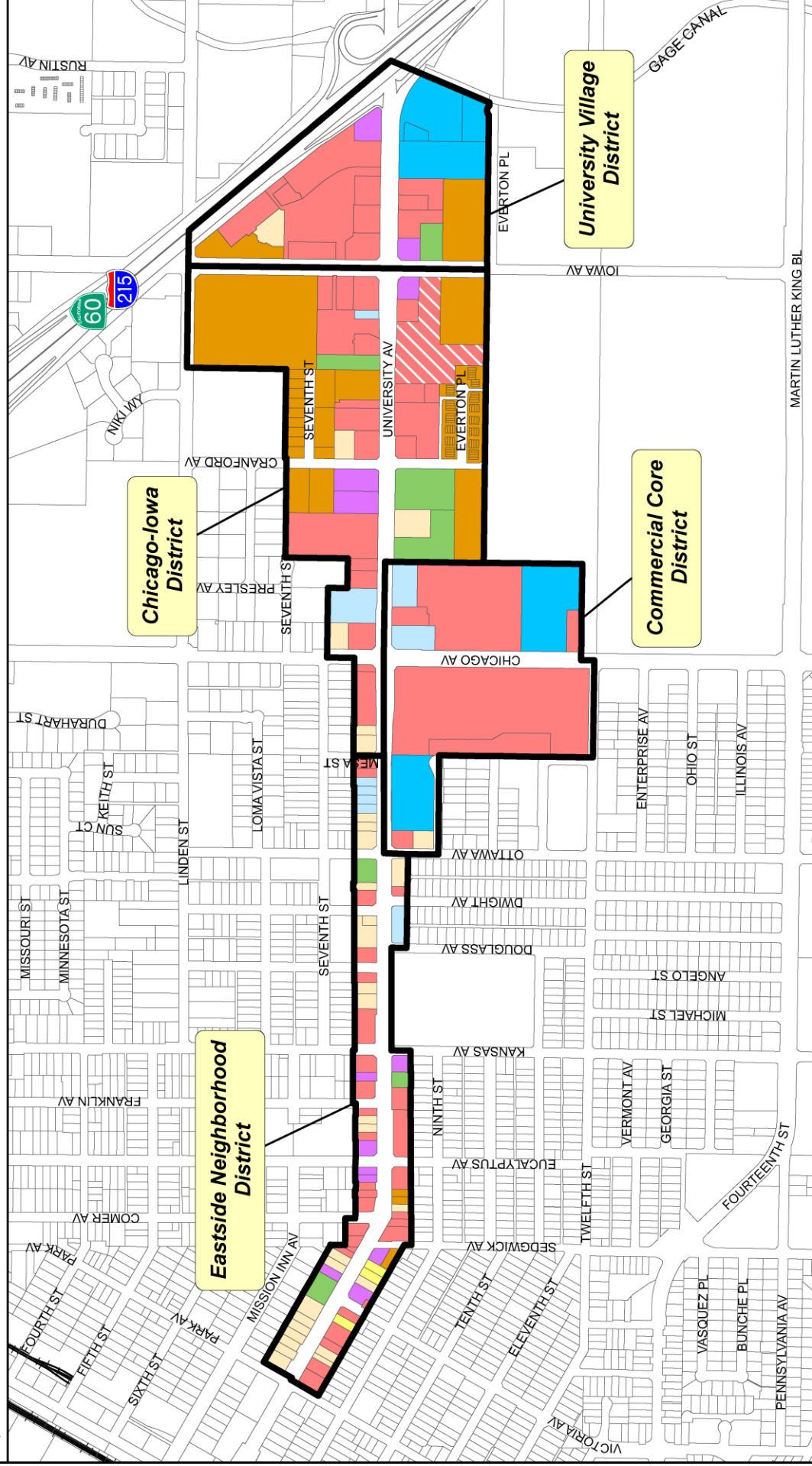
- Specific Plan Boundary

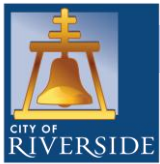




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Figure 11
Existing Land Uses





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The Chicago-Iowa District is primarily located in the University Neighborhood and is the largest of the four Districts and the most diverse in terms of land uses. The western edge of this District contains several neighborhood-serving uses, including restaurants, retail uses, a bank and pharmacy. The center contains multi-family residential uses and two large hotels. The eastern portions contain a mix of uses, including restaurants, office and retail uses, personal services, a vehicle fueling station and several high-density residential developments comprised of multi-family, senior and student housing, as well as a mixed-use development with retail and residential components. This District serves as a transition area between the higher intensity uses located in the University Village District to the east and the smaller-scale uses in the Eastside Neighborhood District to the west.

The University Village District includes the University Village site, a large and integrated development located in the northern portion of the District containing an eight-story student housing development, a six-story parking structure, a two-story multiplex movie theatre and various student-serving uses including restaurants, personal services, office, retail and entertainment uses. The southern portion of the District contains several buildings owned and operated by the University of California, including the UCR Extension Center, as well as a vehicle fueling station, retail uses, multi-family residential and a motel. While not located within the Specific Plan area, the Gage Canal runs immediately adjacent to the west of this District, through the UCR Campus.

3.3 Regional Circulation

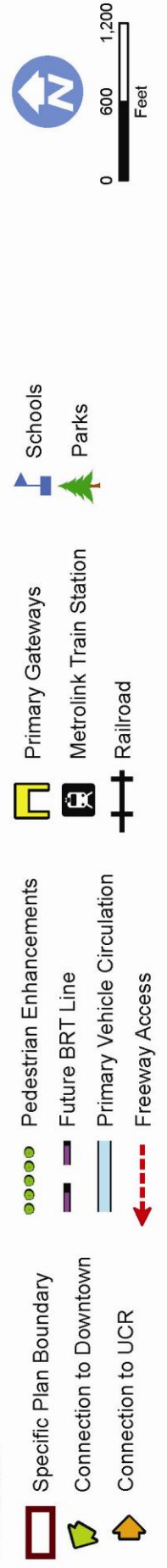
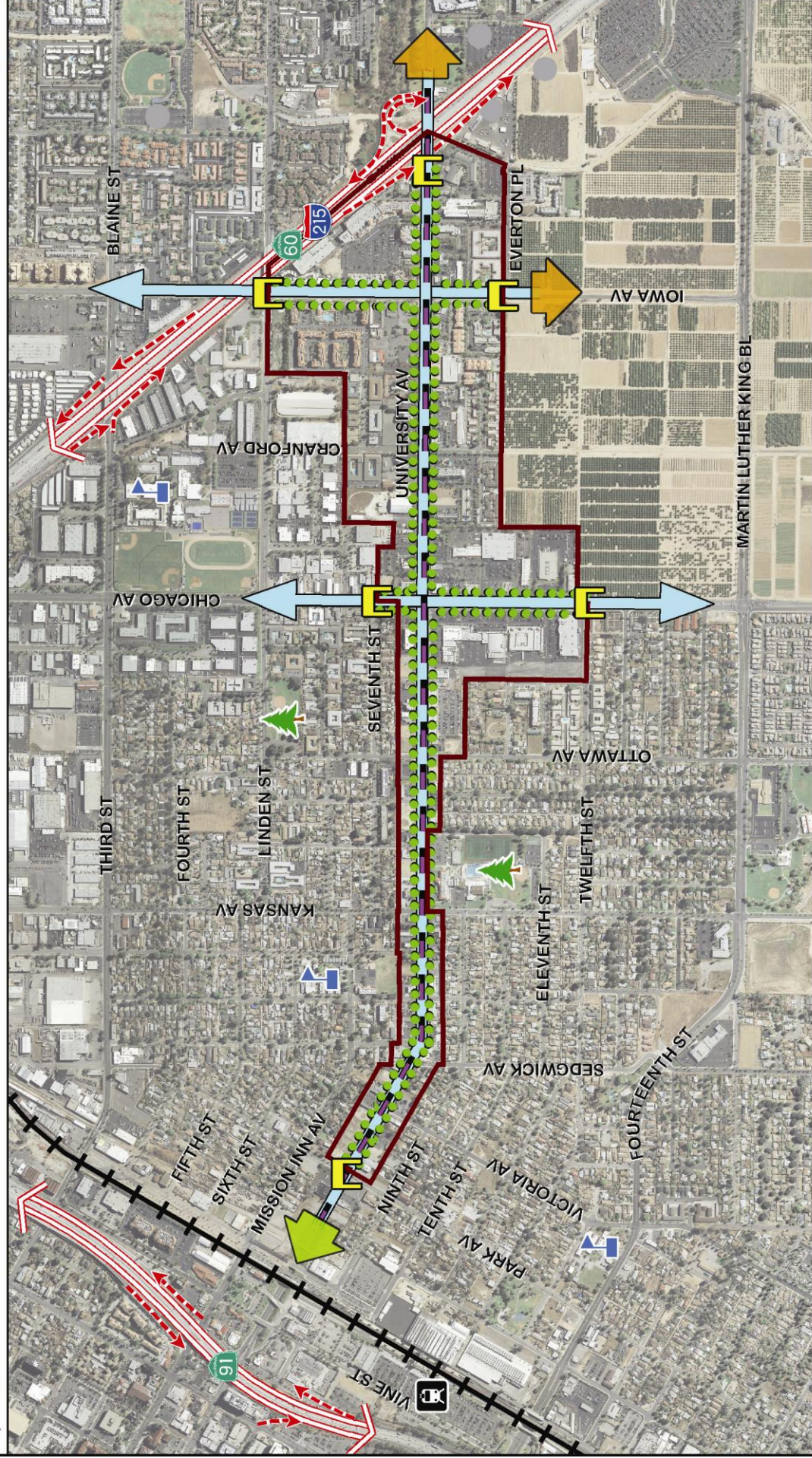
Downtown Riverside Metrolink Station

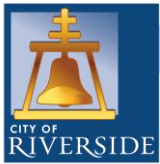


A significant improvement in the history of the area was the development of the Downtown Riverside Metrolink Station in the southern portion of the Marketplace area in 1991. Metrolink trains provide access to San Bernardino, Orange, San Diego, and Los Angeles Counties from Riverside, serving commuters who work in areas outside of the Inland Empire, as well as bringing workers into Riverside. Transit-oriented development is currently taking place in a number of communities in California and there has been significant interest in transit-oriented development for the area around the Downtown Riverside Metrolink Station, including high-density housing, offices, convenience commercial, and restaurants. This is increasingly important with the revitalization of Downtown and the growth of both the Justice Center District (of the Downtown Specific Plan) and the UCR campus that provide opportunities for pedestrian, bicycle, taxi and shuttle bus connections between the Metrolink Station, the Marketplace, Downtown and the UCR campus (Figure 12 – Local Circulation and Linkages).



Figure 12
Local Circulation and Linkages





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UNIVERSITY AVENUE SPECIFIC PLAN

Existing Regional Circulation Network

The University Avenue Specific Plan area is near the crossroads of two major freeway systems. Located to the west is State Route 91 (SR 91), a primary connection between Riverside and Orange/Los Angeles Counties. Interstate 215 (I-215) borders the eastern boundary of the Specific Plan area and connects Riverside to San Diego and intervening cities to the south, and Barstow, Las Vegas and other points to the north. State Route 60 (SR 60) located to the northwest, connects Riverside to Downtown Los Angeles to the west and numerous communities in between. The I-215/SR 60/91 Freeway interchange located to the north of the Specific Plan area.



The California Department of Transportation (Caltrans) completed major construction of the much needed 60/91/215 interchange and flyover just north of the University Avenue Specific Plan area in 2008, relieving a great deal of congestion on all freeways near the Downtown, Marketplace, University Avenue corridor and UCR campus areas. Improvements to I-215 were also completed during this time, including on/off-ramps at Martin Luther King Boulevard and I-215 and the widening of the freeway through the UCR campus. Despite the successful completion of these crucial improvements, Caltrans continues to implement freeway enhancements throughout the City of Riverside, including Downtown. Specifically, Caltrans has begun the construction of new freeway ramps along SR 91 at the eastbound Fourteenth Street and University Avenue off-ramps and westbound Tenth Street on-ramp and Fourteenth Street off-ramp to alleviate merging conflicts. In addition, the westbound Ninth Street on-ramp is to be relocated to Tenth Street. Construction is expected to be completed by 2015.

3.4 Local Circulation

Existing Improvements

Existing Street System

The University Avenue corridor is the centerpiece of the Specific Plan area and the primary thoroughfare connecting the UCR campus to Downtown and the Riverside Marketplace. University Avenue is currently designated as a four-lane 100-foot arterial on the Circulation Element of the General Plan 2025.



While University Avenue has an underpass and on/off ramps at the I-215 Freeway located at the eastern boundary of the Specific Plan area, the Freeway creates a physical and visual barrier between the corridor and the UCR campus. The primary north/south arterials providing access to the University Avenue Specific Plan area are Park Avenue, Chicago Avenue and Iowa Avenue. The following street classifications under the Circulation Element of the Riverside General Plan 2025 are located within the Specific Plan area:

University Avenue	-	100-Foot Arterial (4 Lanes)
Park Avenue	-	88-Foot Arterial (2 Lanes)

3: SPECIFIC PLAN FRAMEWORK



Victoria Avenue	-	66-Foot Collector (2 Lanes)
Kansas Avenue	-	66-Foot Collector (4 Lanes)
Chicago Avenue	-	110-Foot Arterial (4 Lanes)
Iowa Avenue	-	110-Foot Arterial (4 Lanes)
Linden Street	-	80-Foot Collector (4 Lanes)

University Avenue is also identified on the Circulation Element of the General Plan 2025 as both a Scenic Boulevard and a Parkway. In recent years, a major streetscape and median enhancement program was completed along the University Avenue corridor. The highlight of this project was the construction of a raised landscaped median along the entire corridor where a left turn lane had previously run the length of the Avenue.

Existing Vehicle Access

Vehicle access throughout the Specific Plan area has been improved since the previous update to the Specific Plan. The consolidation of lots and development of large projects have reduced the number of curb approaches along the corridor. Further, the recent streetscape and median enhancement program also included the addition of turn pockets at major arterial intersections to alleviate traffic congestion. Finally, the improvements to the 60/91/215 interchange and the I-215 Freeway near the UCR campus, including on/off-ramps at Martin Luther King Boulevard and I-215 and the widening of the freeway, have alleviated traffic along University Avenue.

Existing Pedestrian/Bicycle Access

Since the update to the Specific Plan in 1993, pedestrian and bicycle access along the University Avenue corridor has been marginally improved. Given that University Avenue is the primary thoroughfare connecting the UCR campus to Downtown and the Marketplace, this connection does not present comfortable walking or cycling experiences. While University Avenue and Iowa Avenue have been improved with Class 2 bicycle lanes, the lanes are narrow and partially located within the street gutter. Chicago Avenue, a major arterial within the Specific Plan area, does not currently provide a bicycle lane. Additionally, bicycle amenities including bicycle racks and repair stations, are lacking throughout the Specific Plan area. With respect to pedestrian access, sidewalks are provided throughout the Specific Plan area and provide basic pedestrian connectivity between the four Districts. However, in order to enhance the pedestrian experience and achieve the vision of University Avenue becoming a lively and pedestrian-friendly corridor, the interface between the street and properties along the street frontage needs to be improved to accommodate pedestrians.

Existing and Planned Transit Network

Transit is an important component of the University Avenue corridor and the surrounding areas. The continuing growth of Metrolink, the demand for expanded and enhanced local and long-distance bus service, and the



UNIVERSITY AVENUE SPECIFIC PLAN

need to provide a link between the various modes of transit all speak to the potential for a regional multimodal transit center adjacent to, or in close proximity to, the Metrolink Station. This would focus on bus access to Metrolink, as well as longer distance bus service. This transit center could be integrated with possible expansion of parking at the Metrolink Station. It could also be integrated with a future Riverside Transit Agency (RTA) bus operations facility if appropriate and/or feasible.

Three rail lines traverse the City: the "Inland Empire-Orange County Line," which runs between Riverside and San Juan Capistrano; the "91 Line," which runs from Riverside to downtown Los Angeles via Fullerton and other points in Orange County; and the "Riverside Line," which runs from Riverside to Los Angeles via Ontario and Pomona. Amtrak service is also available at the Riverside-Downtown Metrolink Station. The San Jacinto Branch Line Commuter Rail (Perris Valley Line) Project is a proposed 19-mile extension of the Metrolink 91 Line that would begin at the existing Riverside-Downtown Station and proceed north on the Union Pacific Riverside Industrial Lead tracks for approximately two miles before turning southeast along the San Jacinto Branch Line. The terminus of the Perris Valley Line is in the City of Perris at SR 74 and I-215. The Perris Valley Line will include up to five new stations, operate through three cities (Riverside, Moreno Valley and Perris), as well as directly serve University of California, Riverside and March Air Reserve Base. The project will also provide additional communities such as Hemet, San Jacinto, Murrieta, Lake Elsinore and Temecula closer access to the Southern California commuter rail network.

Local and regional bus service is also provided throughout the City, including service within the University Avenue Specific Plan area, by the Riverside Transit Agency (RTA). Multiple RTA routes traverse the Marketplace area providing direct linkages to other locations throughout Riverside as well as many locations throughout western Riverside County and limited service to Orange and San Bernardino Counties.

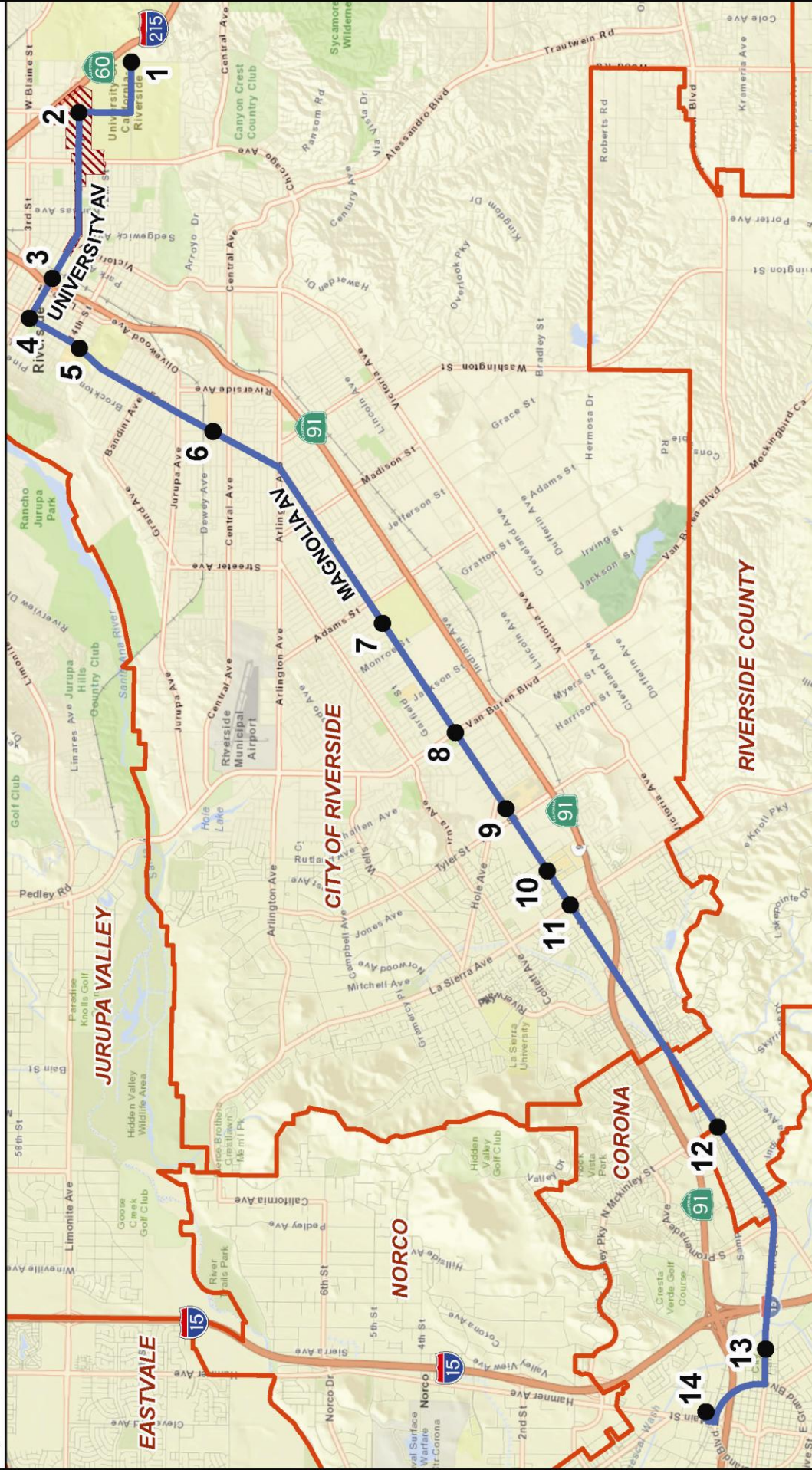
RTA is also in the process of implementing a Bus Rapid Transit (BRT) along the "L" corridor, defined as the corridor along Magnolia Avenue through Market Street and Downtown to University Avenue and terminating on the UCR campus. RTA's new BRT services (named RapidLink) will be integrated into the region's transportation system by connecting with existing local bus, express bus, and rail transit services at key locations. Figure 13 – Proposed Bus Rapid Transit Alignment illustrates the proposed BRT route within the City of Riverside and along University Avenue as well as the potential bus stop locations. One of RapidLink's main design criteria was that it must provide more "rapid" service than contemporary local bus services by having faster average bus travel speeds than comparable local bus routes, and reasonably short wait times at bus stations. These goals are met by operational features such as a skip stop configuration and transit priority merges at RapidLink stations and by technological features such as transit signal priority at signalized intersections.



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Figure 13
Proposed Bus Rapid Transit Route

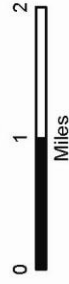


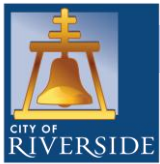
Bus Stops (Source: RTA 2009)

- 1 Canyon Crest Drive at Martin Luther King Blvd
- 2 University Avenue at Iowa Avenue
- 3 Marketplace/Riverside Metrolink
- 4 Downtown Riverside
- 5 Riverside Community Hospital
- 6 Riverside Plaza
- 7 California Baptist University
- 8 Van Buren Blvd
- 9 Galleria at Tyler
- 10 Kaiser Hospital
- 11 La Sierra Avenue
- 12 McKinley Street
- 13 6th Street at Rimpau Avenue
- 14 North Main Corona Metrolink Station

Bus Route

- University Avenue Specific Plan





UNIVERSITY AVENUE SPECIFIC PLAN

Recommended Improvements

Although the University Avenue Specific Plan area features beneficial circulation-related networks and infrastructure elements, there are a number of improvements that would enhance the circulation network in and around the Specific Plan area (Figure 12 – Local Circulation and Linkages). Chapter 4 specifically outlines Objectives and Policies related to the Specific Plan area including, but not limited to, the following recommended improvements:

- Limit driveways and local street access on arterials streets;
- Support implementation of RTA's Bus Rapid Transit Program;
- Enhance pedestrian and bicycle networks;
- Enhance transit circulation network and amenities; and
- Amend the Master Plan of Roadways to designate University Avenue as a business route while accommodating bicycles and pedestrians more easily, and designate Martin Luther King Boulevard as the major east-west thoroughfare in the area.

3.5 Parking

Existing Parking Considerations

There are several issues present within the Specific Plan area that require creative consideration for providing adequate, convenient parking. They are as follows:

- Many of the lots within the Eastside Neighborhood District are substandard in size and will provide little or no room for parking if developed according to the standards of the underlying zone and Specific Plan.
- Street parking is prohibited on all arterials within the Specific Plan area, including University Avenue, Chicago Avenue and Iowa Avenue.
- Existing surface parking areas located along the street frontage reduce the attractiveness of the area as a pedestrian environment.
- The University Village site will require additional parking solutions in order to develop the existing interior surface parking lot as envisioned by the Specific Plan.

Parking Improvements

The numerous adaptive reuse opportunities and potential infill sites located within the Specific Plan area (Discussed in Chapter 4. Also see Figure 16) coupled with the anticipated growth of the UCR campus and the demand for student housing will continue to significantly increase the demand for parking. This demand has been partially addressed with the consolidation of lots within the Eastside Neighborhood District and the construction of parking structures within the Chicago-Iowa and University Village Districts. Within the Commercial Core, Chicago-Iowa and University Village Districts, the provision of multi-level parking structures with reciprocal parking agreements will be crucial in achieving the vision

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outlined in this Specific Plan. Chapter 4 outlines Objectives and Policies that address these issues.

Reciprocal Parking Agreements

New development and adaptive reuse of existing structures within the Specific Plan area should be strongly encouraged to include reciprocal parking agreements with nearby parking areas and structures.

Where reciprocal/shared parking is provided, the Zoning Administrator, City Planning Commission or City Council should allow a reduction in the number of parking spaces where it can be demonstrated that peak parking demands for uses with evening and weekend-oriented activities would be offset by uses with a daytime, weekday peak demand. Parking for residences shall be separated from parking areas provided for commercial and office uses.

Reduction in the number of parking spaces to be provided should be subject to a shared parking analysis prepared by a registered Traffic Engineer and based on the Urban Land Institute (ULI) Shared Parking methodology.

Parking Districts

Although there are currently no parking districts within the University Avenue Specific Plan, it may be beneficial to explore appropriate locations for such a strategy. Parking districts are funding mechanisms used by local governments in defined commercial/business areas to provide centrally- or conveniently-located common parking areas that serve nearby participating businesses. Some of the smaller, higher-density properties along University Avenue may benefit from a parking district that creates an environment that encourages individuals to “park once” and have convenient access to multiple services or businesses in one centralized location. Much like Business Improvement Districts, the cost of maintaining and operating the parking facilities is paid for by the participating businesses.

Chapter 8 includes a discussion on ways to establish parking districts as well as a brief description of the steps necessary.

3.6 Public Infrastructure

Infrastructure and Utilities

Figure 14 – Existing Sewer Facilities and Figure 15 – Existing Water Infrastructure depict existing infrastructure within the University Avenue Specific Plan area. Because the area is largely developed, it remains relatively well served by water and sewer infrastructure and will likely meet the needs of any future development within the Specific Plan area. Service capacity will be analyzed on a case by case basis as projects are considered. It should be noted that each developer will be responsible for



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any costs associated with the extension of lateral utility lines on their property and any related connection fees.

Water and electric service within the Specific Plan area is provided by Riverside Public Utilities (RPU), wastewater service within the Specific Plan area is provided by The City of Riverside Public Works Department, and natural gas is provided by the Southern California Gas Company.

Gage Canal

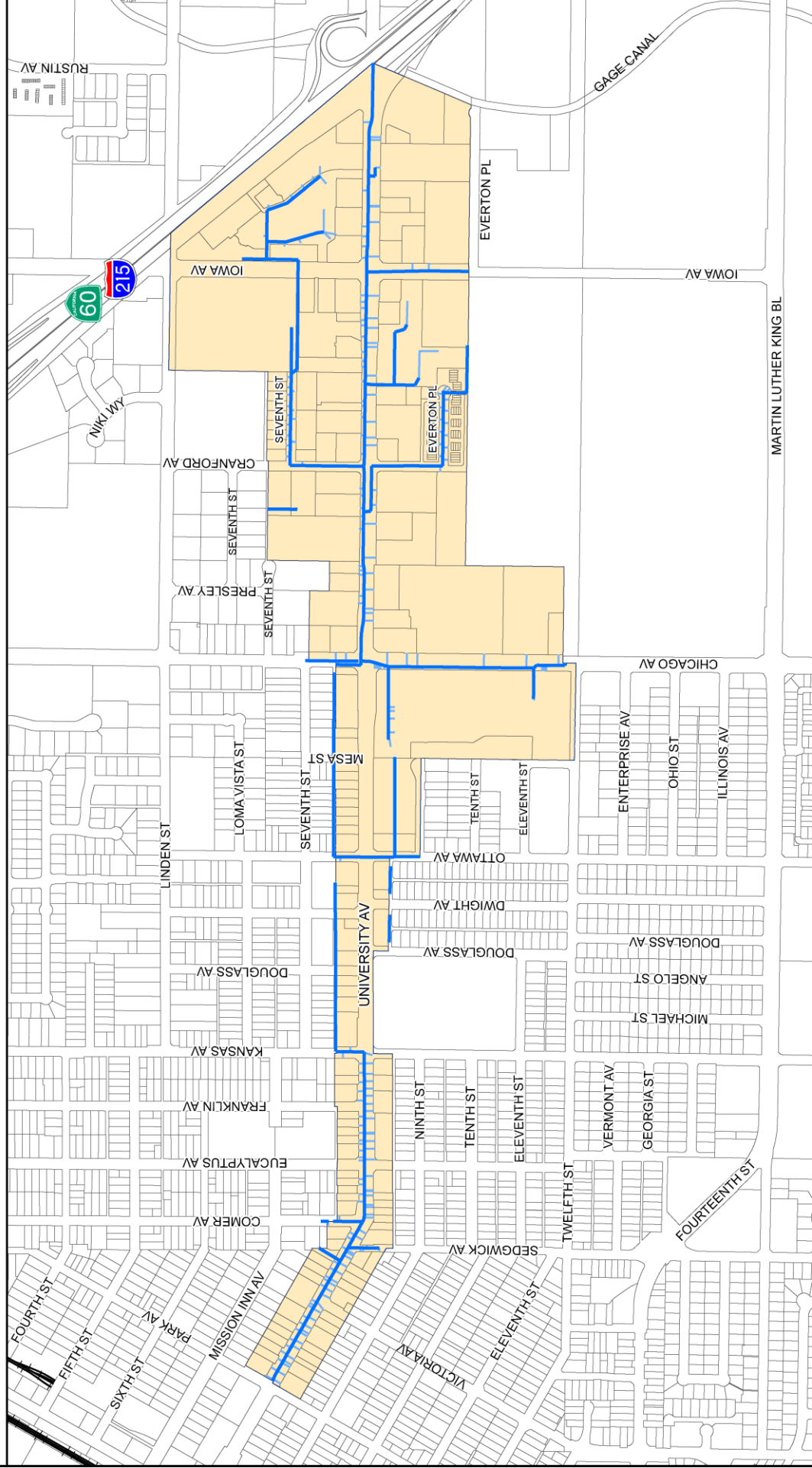
The United States Geological Survey (USGS) topographic maps depict a blue-line stream feature extending along the eastern boundary of Specific Plan area. While not located within the Specific Plan area, this feature is a segment of the Gage Canal, a historic agricultural canal that was chiefly responsible for the proliferation of the citrus industry in Southern California. The canal extends through the UCR Campus and the City of Riverside and is primarily an open trench canal through the campus, except for the portions that extend north of the I-215 Freeway. See Figure 15 for location of the canal.



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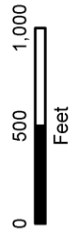


Figure 14
Existing Sewer Facilities



— Sewer Main

— University Avenue Specific Plan

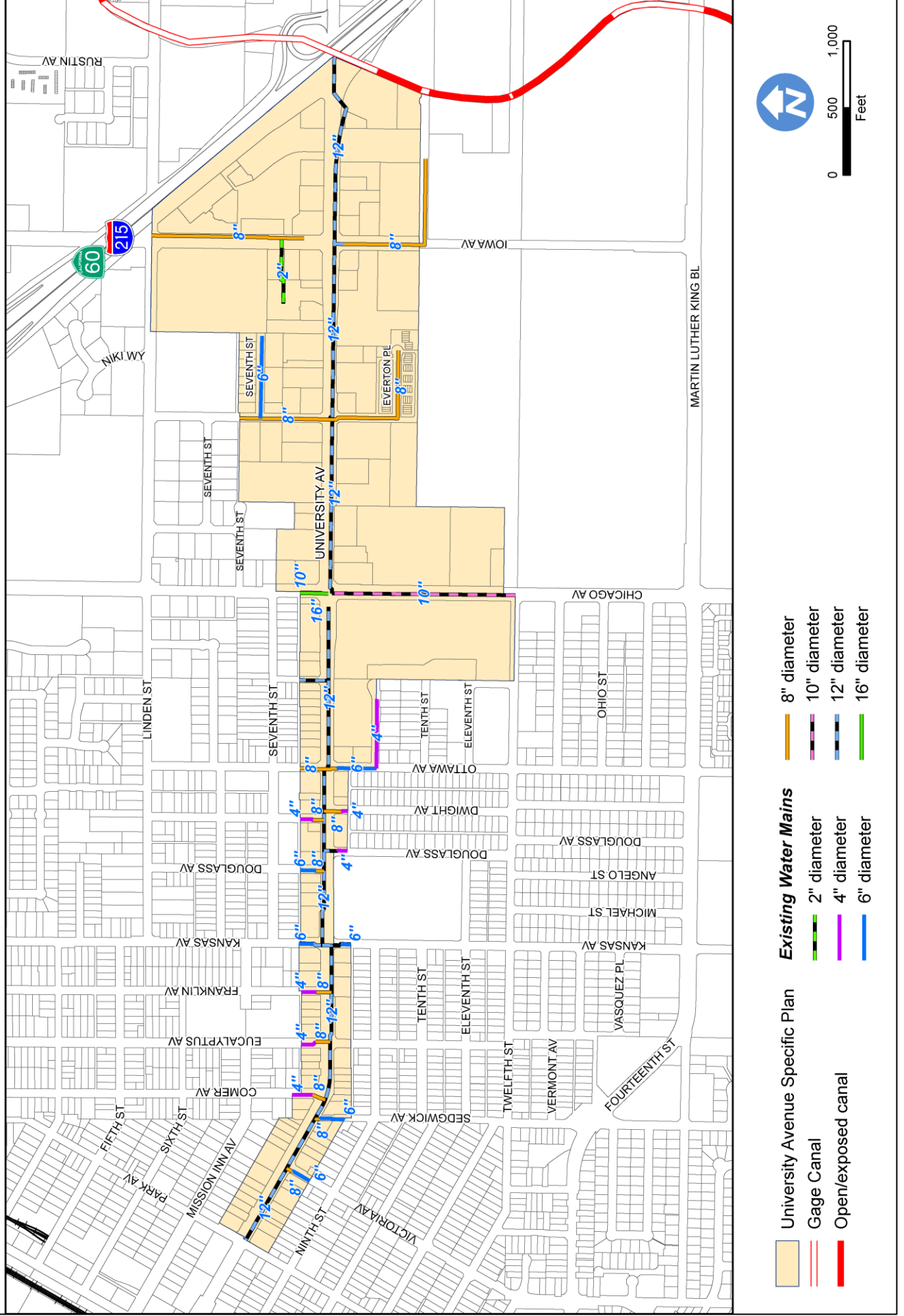




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Figure 15
Existing Water Infrastructure





4

Chapter 4: Visioning University Avenue

4.1 Opportunities and Constraints

The General Plan 2025 identifies University Avenue as an important arterial within the City of Riverside and features many Objectives and Policies that specifically address the University Avenue Corridor and related development and enhancements. The General Plan 2025 also identifies University Avenue as part of the “L” Corridor along with Magnolia Avenue and Market Street, acting as a link to a series of activity centers and offering important opportunities for multi-modal transportation linkages and pedestrians (See Figure 16).

The University Avenue corridor is the primary link between the University of California, Riverside (UCR) and Downtown and features freeway access from I-215/SR-60 and SR-91, with Martin Luther King Boulevard serving as the primary vehicular cut-thru access between the two freeways. These factors alone contribute to the potential for University Avenue to become a vital and economically significant part of Riverside’s future.

Recognizing the potential for the Avenue, the Redevelopment Agency implemented an aggressive and extensive property acquisition program between 2005 and 2011 that resulted in the acquisition of many dilapidated properties and the elimination of many undesirable uses along University Avenue. Through this effort, the Redevelopment Agency was able to assemble multiple properties and cleared many of the properties via building relocations and demolitions, providing future developers greater opportunity and flexibility. However, in 2011 the California Legislature enacted the Dissolution Act, effectively dissolving all Redevelopment Agencies in the State as of February 1, 2012. Nonetheless, the potential infill sites identified on Figure 16 include these sites along with adaptive reuse opportunities and major projects completed since the 1993 update to the Specific Plan.

As a result of these efforts and the inherent potential related to the location and setting of the corridor, University Avenue is poised to become a prominent commercial and mixed-use corridor with a strong network of interconnected modes of transportation and street-focused development featuring pedestrian-scale amenities. In an effort to capitalize on the development potential for University Avenue, the City, with the help of the Citizens Advisory Committee, have established a framework of opportunities and constraints that influence the future development of the University Avenue Specific Plan area. These opportunities and constraints also influenced the development of this Specific Plan.

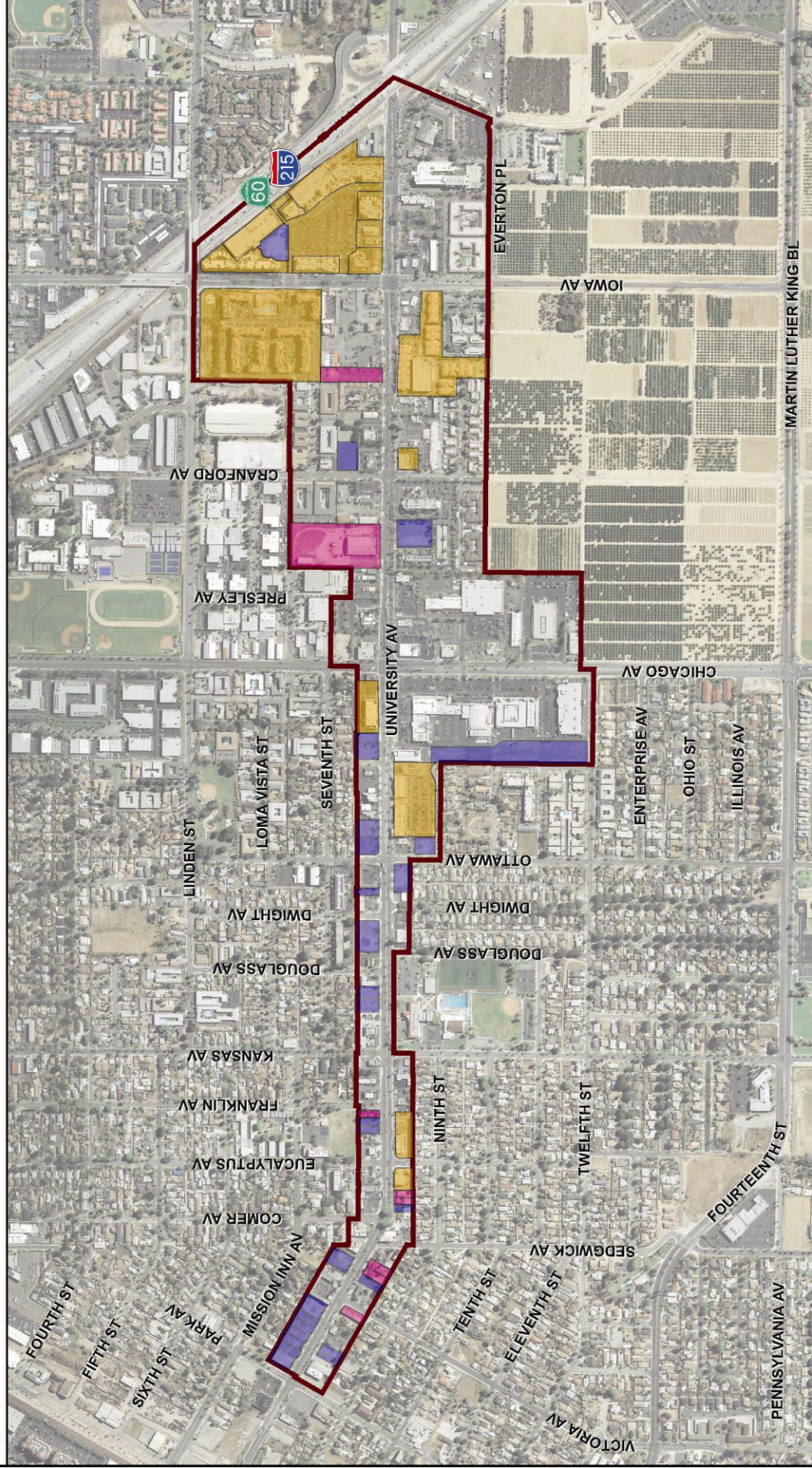




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Figure 16
Opportunity Sites



2012 Aerial Photo

- Major Projects Implemented under University Avenue Specific Plan
- Adaptive Reuse Opportunities
- Potential Infill
- Specific Plan Boundary



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Feet

4: VISIONING UNIVERSITY AVENUE

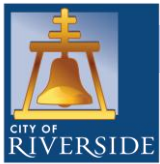


Opportunities

- The availability of vacant and underutilized land along the University Avenue corridor provides opportunities to attract new residential and commercial development.
- The continued growth of UCR is increasing the demand for student housing and nearby dining and entertainment venues for students, faculty, and staff.
- The future completion of the Metrolink Perris Valley Line with a stop in the adjacent Marketplace Specific Plan area will increase transit ridership and demand for well-linked multi-modal transportation networks including bicycle and pedestrian connections along University Avenue.
- The future completion of UCR's Long Range Development Plan, particularly the development of the Medical School on the western portion of campus, will offer opportunities to provide enhanced pedestrian linkages between campus and the University Avenue corridor.
- Facilitating compact, mixed-use developments and expanding and improving pedestrian and transit facilities and connectivity will contribute to reductions in vehicle miles traveled (VMT) and greenhouse gas (GhG) emissions.
- There are continued opportunities to enhance the transit links to nearby major employment centers, including, but not limited to Hunter Business Park and Downtown.
- Enhancements to the bicycle network along University Avenue could provide an improved linkage between UCR and the cultural attractions Downtown.
- As part of the Riverside Renaissance (a \$1.57 billion investment approved in 2006 to complete 30 years of capital projects in five years), extensive improvements were made to University Avenue, including the addition of raised, landscaped center medians, enhanced parkway landscaping, and the installation of a number of mature palm trees, effectively unifying and improving the visual appeal of the entire corridor.

Constraints

- The properties in the Eastside Neighborhood District, generally between Park Avenue and Mesa Street, feature shallow lots with an average depth of 150 feet, and some as little as 104 feet, resulting in challenges for new development in this area.



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- Existing undesirable uses are not consistent with the long-term Vision for the University Avenue Specific Plan area and may deter development of adjacent or nearby properties.
- Despite the Redevelopment Agency's efforts to eliminate undesirable uses in the last few years, there still remains a proliferation of existing auto-oriented uses such as drive-thru restaurants, vehicle repair facilities, auto parts stores, and motels that are not consistent with the overall Vision for the University Avenue Specific Plan.
- University Avenue remains a primary arterial for cut-thru traffic between 1-215/SR-60 and SR-91. Accommodating the existing level of service for vehicular traffic while enhancing and expanding the pedestrian and bicycle networks creates challenges and potential conflicts.
- Many properties within the Eastside Neighborhood District are adjacent to or backup to existing single-family areas, posing potential compatibility issues for any new development along University Avenue.
- Safety and security issues are a major concern for nearby residents, particularly in the alleys that separate the single-family residences from the commercial and office uses.
- Despite recent efforts to create a unified landscape pallet in the public right-of-ways, the ever-evolving development and reinvention of University Avenue over the course of nearly 100 years has resulted in a development pattern with an inconsistent visual character, with little or no continuity in the quality or design of architecture and private landscaping.
- Inconsistent signage standards and enforcement over many decades have resulted in an overabundance of undesirable signs, creating a cluttered "sign blighted" view shed in many locations along University Avenue.
- The existing development pattern of many small, individual commercial lots with no inter-connectivity creates an excess of driveways and curb cuts that can pose a cumulative hazard to other vehicles, pedestrians, and bicycles.

4.2 Vision

Overall Vision for University Avenue

Through community outreach efforts related to this Specific Plan as well as previous planning efforts, a clear Vision for the University Avenue Corridor has been defined. As such, it is the intent of the University Avenue Specific

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Plan to facilitate and guide future development through clear objectives, policies, development standards and design guidelines consistent with the Vision. The following Vision is written from a future perspective in an effort to provide a glimpse at what the Avenue could become upon the successful implementation of the standards and guidelines that follow within this Specific Plan:

University Avenue Specific Plan Vision

University Avenue is an active and lively Avenue, with a variety of housing, restaurants, retail businesses, and public facilities, serving both the immediate residents and businesses, as well as being a destination of choice for people throughout the City.

University Avenue has become an important business route between State Route 91 and Interstate 215/State Route 60 providing a link between the Downtown and the University of California-Riverside, through the Eastside and University Neighborhoods.

While cars are still welcome, priority has been given to public transportation, bicycles and walkability.

By focusing on pedestrian connectivity and an urban design fabric that has brought buildings closer to the street and deemphasized the automobile, the Avenue has naturally evolved into four separate but interrelated "districts" that reflect the unique character of each neighborhood, yet have a continuity that spans the entire Avenue.

District Visions

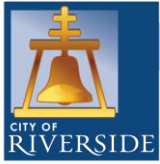
In addition to the overall Vision for University Avenue, visions were also developed for each of the four districts within the University Avenue Specific Plan:

District 1:

- The character of the District has been transformed into a vibrant mix of neighborhood-serving retail, office, and residential uses while remaining sensitive to existing historic properties and surrounding single-family residential areas.
- The small, shallow lots have been consolidated to create attractive and cohesive developments that have a strong street presence, promote walkability, and feature wide sidewalks and shared parking in the rear.

District 2:

- District 2 has benefited from the large lots that have allowed the flexibility to create larger scale developments with integrated residential, office and retail uses.



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- Streetscape and landscape enhancements, as well as improved pedestrian connectivity across Chicago Avenue, have created an elevated aesthetic appeal and a safer pedestrian environment, while providing an incentive for new businesses with a broader appeal.

District 3:

- The larger lots of District 3 have allowed the area to develop with a mix of uses, with an emphasis on residential and entertainment uses and the proximity of the District to UCR has been capitalized upon through the development of uses serving the UCR community and surrounding neighborhoods, particularly at the eastern end of the District.
- The development along the Avenue features buildings that sit close to the street and are larger in scale, yet the retail and entertainment uses on the first floor, along with wider sidewalks, public gathering spaces, and street furniture, create a vibrant and inviting pedestrian-scaled environment.

District 4:

- Capitalizing on the proximity to UCR, District 4 has become a desirable destination for student-oriented dining, shopping and entertainment opportunities as part of larger high-density mixed-use developments that also feature prominent residential components.
- The south side of University Avenue has been transformed into a cohesive mixed-use development through lot consolidation and benefits from the enhanced pedestrian connections between UCR and University Village.
- The emphasis on creating a pedestrian-friendly environment is evident in the street-oriented buildings, wide sidewalks, interconnectivity between developments, public gathering spaces, and other pedestrian amenities.

4.3 Objectives and Policies

The University Avenue Specific Plan supports and sets in motion a number of the Objectives and associated Policies established within the General Plan 2025. Specifically, the University Avenue Specific Plan places an emphasis on infill development, adaptive reuse of existing structures, connectivity and walkability, access to multi-modal transportation opportunities, and the use of mixed-use development where appropriate. Additionally, an important concept introduced in this Specific Plan is an emphasis on clean and green building techniques and there are several Citywide Objectives and related Policies within the General Plan 2025, and augmented by the University Avenue Specific Plan, that directly address this concept.

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The following Objectives and Policies begin to implement the Vision and illustrate the Smart Growth principles and Clean and Green strategies outlined in the General Plan 2025 that should be considered as part of any future efforts within the University Avenue Specific Plan area. Objectives and Policies derived from the General Plan 2025 Land Use and Urban Design Element, Circulation and Community Mobility Element, Historic Preservation Element, Public Safety Element and Air Quality Element are identified with the designation "LU," "CCM," "HP," "PS," or "AQ," respectively, while those derived from the Eastside Neighborhood Plan or University Neighborhood Plan are designated with "ENP" or "UNP," accordingly. Those Objectives and Policies unique to the University Avenue Specific Plan are identified with the designation "UASP" and feature the University Avenue Specific Plan diamond logo:

Land Use Objectives and Policies

Objective LU-8: Emphasize Smart Growth principles through all steps of the land development process.

Policy LU-8.1: Ensure well-planned infill development Citywide, allow for increased density in selected areas along established transportation corridors.

Policy LU-8.2: Avoid density increases or intrusion of non-residential uses that are incompatible with existing neighborhoods.

Policy LU-8.3: Allow for mixed-use development at varying intensities at selected areas as a means of revitalizing underutilized urban parcels.

Policy LU-8.4: Ensure that infill development and development along Magnolia and University Avenues incorporates the latest Smart Growth principles.



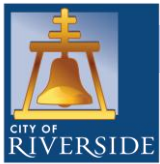
Policy UASP 1: Encourage shared driveways between adjacent properties and shared parking opportunities between land uses with opposite peak usage.



Policy UASP 2: Promote walkability within the University Avenue Specific Plan area by requiring or encouraging amenities such as wide sidewalks, covered walkways or arcades, street furniture, lighting, contrasting paving at crosswalks, and the like through the use of clear development standards and design guidelines.



Policy UASP 3: Encourage the implementation of public-private open space with a combination of landscaping



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and hardscape surfaces for outdoor seating or dining in the front setback areas of commercial and mixed-use developments.



Policy UASP 4: Coordinate with UCR to ensure that the future development of the West Campus, including the future School of Medicine, provides integrated pedestrian connections to the University Avenue corridor.



Policy UASP 5: Ensure pads are graded and buildings are designed in such a way that they provide adequate barrier-free access to and from the sidewalk along the Avenue.

Objective LU-9: Provide for continuing growth within the General Plan area, with land uses and intensities appropriately designated to meet the needs of anticipated growth and to achieve the community's objectives.

Policy LU-9.3: Designate areas for urban land uses where adequate urban levels of public facilities and services exist or are planned, in accordance with the public facilities and service provision policies of this General Plan.

Policy LU-9.5: Encourage the design of new commercial developments as "integrated centers," rather than as small individual strip development. Integrate pedestrian access, parking, access, building design and landscape themes across all parcels in the commercial center to unify the development.

Policy LU-9.6: Discourage strip commercial development and encourage a pattern of alternating land uses along major arterials with "nodes" of commercial development separated by other uses such as residential, institutional or office.

Objective LU-14: Recognize that University Avenue serves as a major parkway linking neighborhoods with such major components of "Riverside Park" as UCR open space areas, Box Springs Mountain Regional Park, the Main Street Pedestrian Mall, White Park, Mt. Rubidoux Park and Fairmount Park.

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Policy LU-14.1: Revise the University Avenue Specific Plan to better reflect the expanded role of this thoroughfare, providing for completion of appropriate streetscape improvements and adjoining land uses.

Objective ENP 3: Enhance the University Avenue corridor by providing opportunities to revitalize commercial areas with and appropriate mix of home ownership opportunities, office, neighborhood-serving retail, and community-based businesses consistent with the General Plan 2025.

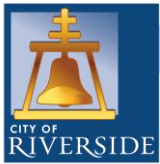
Policy ENP 3.1: Encourage street-oriented mixed-use development of an appropriate scale and intensity to catalyze and encourage further improvement to the entire Eastside Neighborhood.

Policy ENP 3.2: Explore opportunities to further develop a streetscape enhancement plan as part of the University Avenue Specific Plan that encourages the extensive use of pedestrian friendly elements such as wide walkways, shade trees, benches, etc. and removes existing impediments to walkways, sidewalks and parkways.

Objective UNP 3: Enhance the University Neighborhood's quality of life by protecting single family area, providing quality, affordable housing and enhancing neighborhood shopping.

Policy UNP 3.2: Provide quality, affordable housing for University Neighborhood residents, students, faculty and staff.

Policy UNP 3.3: Explore opportunities to revitalize older shopping centers by increasing the level of neighborhood shopping and pedestrian amenities, upgrading the tenant mixes and encouraging private sector investment in the existing shopping centers throughout the University Neighborhood. This may include introducing mixed-use housing where appropriate.



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Policy UNP 3.4: Continue to upgrade University Avenue as a location for community and University related housing and commercial areas as well as enhancing University Neighborhood's accessibility to Downtown and the Riverside Marketplace.

Objective UNP 4: Ensure coordinated development along University Avenue in recognition of the street's importance as a key route between the University and the Downtown Core, a center for the hospitality industry and a source of commercial services for surrounding residential neighborhoods.

Policy UNP 4.1: Update the University Avenue Specific Plan to allow for mixed-use and residential development along the corridor that supports land use designations of the General Plan.

Policy UNP 4.2: Encourage the creation of a continuous uniform streetscape along University Avenue.

Policy UNP 4.3: Encourage student housing and activities along the University Avenue corridor.

Historic Preservation Objectives and Policies

Objective HP-1.0: To use historic preservation principles as an equal component in the planning and development process.

Policy HP-1.5: The City shall promote neighborhood/city identity and the role of historic preservation in community enhancement.

Policy HP-1.6: The City shall use historic preservation as a tool for "Smart Growth" and mixed use development.

Objective HP-2.0: To continue an active program to identify, interpret and designate the City's cultural resources.

Policy HP-2.1: The City shall actively pursue a comprehensive program to document and preserve historic buildings, structures, districts, sites (including archaeological sites), objects, landscapes, and natural resources.

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Policy HP-2.2: The City shall continually update its identification and designation of cultural resources that are eligible for listing in local, state and national registers based upon the 50 year age guideline for potential historic designation eligibility.

Objective HP-5.0: To ensure compatibility between new development and existing cultural resources.

Policy HP-5.1: The City shall use its design and plot plan review processes to encourage new construction to be compatible in scale and character with cultural resources and historic districts.

Policy HP-5.2: The City shall use its design and plot plan review processes to encourage the compatibility of street design, public improvements, and utility infrastructure with cultural resources and historic districts.

Objective HP-7.0: To encourage both public and private stewardship of the City's cultural resources.

Policy HP-7.2: The City shall incorporate preservation as an integral part of its specific plans, general plan, and environmental processes.

Transportation and Circulation Objectives and Policies

Objective CCM-1: Facilitate freeway and regional roadway improvements and construction to alleviate congestion and air pollution and to minimize regional cut-through traffic within Riverside.

Policy CCM-1.2: Support the addition of capacity improvements to S-91, SR-60, I-215 and I-15

Objective CCM-2: Build and maintain a transportation system that combines a mix of transportation modes and transportation system management techniques, and that is designed to meet the needs of Riverside's residents and businesses, while minimizing the transportation system's impact on air quality, the environment and adjacent development.



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Policy CCM-2.7: Limit driveway and local street access on Arterial Streets to maintain a desired quality of traffic flow. Wherever possible, consolidate driveways and implement access controls during redevelopment of adjacent parcels.

Policy CCM-2.10: Emphasize the landscaping of parkways and boulevards.

Objective CCM-8: Protect neighborhoods and reduce the risk posed to young children and other residents by vehicular traffic on local roadways.

Policy CCM-8.5: Continue to participate in the Riverside County Transportation Commission's SB 821 program for the funding of facilities for the exclusive use of pedestrians and bicyclists to eliminate missing sidewalk and/or bicycle path links.

Objective CCM-9: Promote and support an efficient public multi-modal transportation network that connects activity centers in Riverside to each other and to the region.

Policy CCM-9.1: Encourage increased use of public transportation and multi-modal transportation as means of reducing roadway congestion, air pollution and non-point source water pollution, through such techniques as directing new growth along transportation corridors.

Policy CCM-9.2: Support implementation of RTA's Bus Rapid Transit Program and recommendations of the Go Riverside Task Force.

Policy CCM-9.6: Enhance and encourage the provision of attractive and appropriate transit amenities, including shaded bus stops, to facilitate use of public transportation, through the development process by incorporating the necessary design features as appropriate.

Policy CCM-9.7: Ensure adequate connections among all alternative modes.

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Objective CCM-13: Ensure that adequate on- and off-street parking is provided throughout Riverside.

Policy CCM-13.2: Accommodate joint use of parking facilities as part of an area plan or site plan, based on the peak parking demands of permitted uses in the planning area.



Policy UASP-7: Implement development standards and incentives that reduce the overall parking demand, including, but not limited to:

- Locating housing near transit stops;
- Requiring developers/owners of new multiple-family residential units to provide bus/train passes to residents;
- Encouraging developers/owners to explore implementation of on-site car-sharing programs; and
- Incorporating public parking into larger scale private developments.

Objective ENP 8: Enhance the Eastside Neighborhood's quality of life by improving parking and traffic problems and all modes of transportation including sidewalks, bike routes, and public transportation.

Policy ENP 8.1: Enhance walkability and bike routes throughout the Eastside Neighborhood.

Policy ENP 8.2: Support the efforts of RTA, RCTC, and others to analyze opportunities for enhanced and expanded transit routes, hours of operation, and bus stop or rail station locations to better serve the Eastside residents.

Public Safety Objectives and Policies

Objective PS-5: Provide safe pedestrian and bicyclist environments Citywide.

Policy PS-5.1: Enhance and maintain pedestrian safety through the inclusion of well-designed streets, sidewalks, crosswalks, traffic control devices and school routes throughout the City. Reasonable means of pedestrian accessibility shall be an important consideration in the approval of new development.



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Policy PS-5.2: Prioritize locations for potential pedestrian safety enhancements, including modified signage, lighted crosswalks and other similar facilities.

Policy PS-5.4: Require that new development provide adequate safety lighting in pedestrian areas and parking lots.

Policy PS-5.5: Implement pedestrian and bicycle safety measures in any new grade separation project.



Policy UASP-8: Widen the University Avenue/freeway underpass sidewalks to the maximum extent feasible.



Policy UASP-9: Where possible, raise the grade of the sidewalk and add safety railings in underpasses to the maximum height possible to further separate pedestrian and vehicular traffic, creating a greater sense of safety.



Policy UASP-10: Explore opportunities to enhance and expand the bicycle network consistent with the Bicycle Master Plan throughout the University Avenue Specific Plan area to strengthen the connectivity to Downtown and other areas of the City.

Sustainability and Energy Efficiency Objectives and Policies

Objective AQ-1: Adopt land use policies that site polluting facilities away from sensitive receptors and vice versa; improve job-housing balance; reduce vehicle miles traveled and length of work trips; and improve the flow of traffic.

Policy AQ-1.5: Encourage infill development projects within urbanized areas, which include job centers and transportation nodes.

Policy AQ-1.6: Provide a mechanism to create opportunities for mixed-use development that allows the integration of retail, office, institutional and residential uses for the purpose of reducing costs of infrastructure construction and maximizing the use of land. See Policy AQ-1.12.

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Policy AQ-1.12: Support mixed-use land use patterns, but avoid placing residential and other sensitive receptors in close proximity to businesses that emit toxic air contaminants to the greatest extent possible. Encourage community centers that promote community self-sufficiency and containment and discourage automobile dependency. See *Policy AQ-1.6*.

Policy AQ-1.15: Establish land use patterns that reduce the number and length of motor vehicle trips and promote alternative modes of travel.



Policy UASP-11: Design roadways to accommodate all modes of travel for all users equally including bicyclists, pedestrians, automobiles, or in other words, support Complete Streets.

Objective AQ-5: Increase energy efficiency and conservation in an effort to reduce air pollution.

Policy AQ-5.7: Require residential building construction to meet or exceed energy use guidelines in Title 24 of the California Administrative Code.



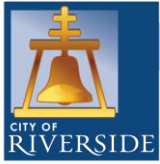
Policy UASP-12: Highlight and promote green building incentives and encourage green building techniques that go above and beyond the minimum Title 24 and Green Building Code requirements.

Objective AQ-8: Make sustainability and global warming education a priority for the City's effort to protect public health and achieve state and federal clean air standards.

Policy AQ-8.24: Meet the environmentally sensitive goals of the General Plan 2025 specified in the Mitigation Monitoring Program of the Program Environmental Impact Report, and the Implementation Plan following the timelines set forth in each.



Policy UASP-13: Promote University Avenue as a Solar Specific Plan through the implementation of programs for residential and commercial customers that will increase solar generation.



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Policies UASP-15 and UASP-16 are consistent with, and implement, the City of Riverside Green Action Plan.



Policy UASP-14: Encourage new construction within the University Avenue Specific Plan area to implement load shifting to off-peak hours.



Policy UASP-15: Encourage that a minimum of 90% of the waste from all construction sites throughout the University Avenue Specific Plan area be recycled.



Policy UASP-16: Require a 5% minimum reduction of disposable, toxic or nonrenewable construction-related products.



Policy UASP-17: Apply urban planning principles that encourage higher density, mixed use, walkable/bikeable neighborhoods, and coordinate land use and transportation with open space systems throughout the University Avenue Specific Plan area.



Policy UASP-18: Promote the concept of urban nature by requiring new development within the University Avenue Specific Plan area to incorporate the planting of shade trees within the development as well as in adjacent parkways as appropriate.



Policy UASP-19: Ensure traffic signals are synchronized throughout the University Avenue Specific Plan area.



Policy UASP-20: Encourage the use of bicycles as an alternative form of transportation, not just recreation, by requiring new development to analyze and implement ways to integrate connectivity between the project and the greater bicycle trail network.



Policy UASP-21: Promote and encourage the use of alternative methods of transportation throughout the University Avenue Specific Plan area by incentivizing development-based neighborhood electric vehicle (NEV) programs, shared vehicle programs, and enhanced bicycle amenities.

Objective OS-8:	Encourage the efficient use of energy resources by residential and commercial users.
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- Policy OS-8.2: Require incorporation of energy conservation features in the design of all new construction and substantial rehabilitation projects pursuant to Title 24, and encourage the installation of conservation devices in existing developments.
- Policy OS-8.4: Incorporate solar considerations into development regulations that allow existing and proposed buildings to use solar facilities.
- Policy OS-8.5: Develop landscaping guidelines that support the use of vegetation for shading and wind reduction and otherwise help reduce energy consumption in new development for compatibility with renewable energy sources (i.e., solar pools).
- Policy OS-8.6: Require all new development to incorporate energy efficient lighting, heating and cooling systems pursuant to the Uniform Building Code and Title 24.
- Policy OS-8.7: Encourage mixed use development as a means of reducing the need for auto travel.
- Policy OS-8.8: Encourage the use of clean burning fuels and solar energy for space and water heating purposes and explore ways to participate in California New Solar Homes Partnerships.
- Policy OS-8.9: Encourage construction and subdivision design that allows the use of solar energy systems.
- Policy OS-8.10: Support the use of public transportation, bicycling and other alternative transportation modes in order to reduce the consumption of non-renewable energy supplies.
- Policy OS-8.11: Support public education programs for City residents and businesses to provide information on energy conservation and on alternatives to non-renewable energy sources.
- Policy OS-8.12: Require bicycle parking in new non-residential development.



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Healthy Lifestyle Objectives and Policies

Objective UASP-1: Improve the health of Riverside's residents and strive to reduce obesity through design, education, and policy-level actions.

Objective UASP-1: and Policies UASP-22 through UASP-25: strive to implement the goals of the Mayor's Fit, Fresh & Fun Forum.



Policy UASP-22: Encourage employers to implement programs that promote healthy and active lifestyles with incentives for healthy living.



Policy UASP-23: Explore policies that incentivize industries to transition from unhealthy practices to healthy practices, for example, restaurants and food manufacturers.



Policy UASP-24: Allow for and facilitate the establishment of community gardens throughout the residential areas of the University Avenue Specific Plan area.



Policy UASP-25: Coordinate with Riverside Unified School District to establish a healthy kids program in schools located in the vicinity of the Specific Plan area.

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Chapter 5: Developing Green on University Avenue

5.1 Purpose

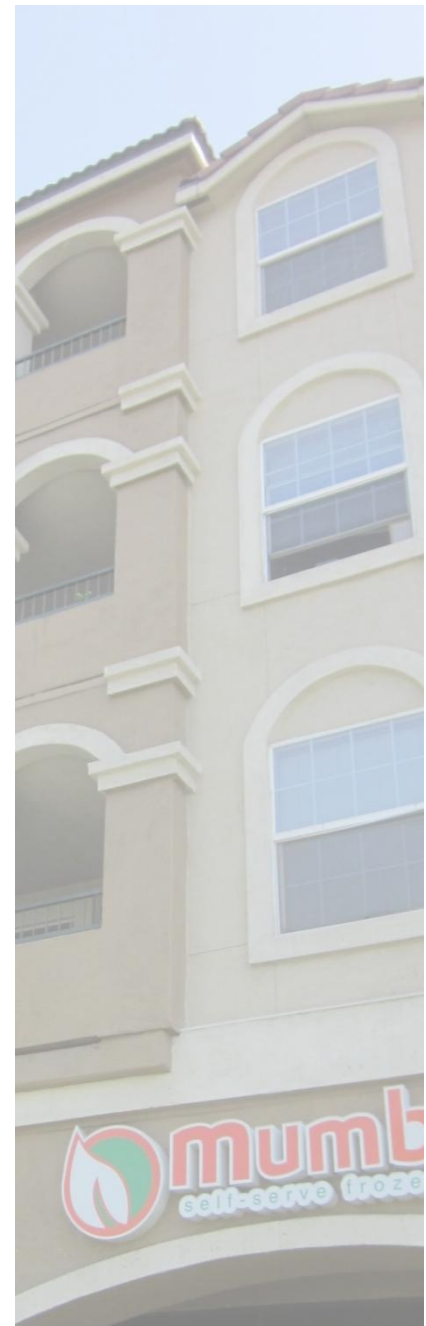
As the first California city to be designated an Emerald City by the California Department of Conservation, the City of Riverside is fast becoming a leader in clean and green practices. The City's adopted Green Action Plan/Emerald City Plan provides the framework to ensure that the City continues this trend. The Plan includes goals and strategies related to energy, greenhouse gas emissions, waste, urban design, urban nature, transportation, water and healthy communities. The University Avenue Specific Plan provides the perfect opportunity to implement the Green Action Plan/Emerald City Plan. Beyond the inclusion of objectives and policies reflecting the Green Action Plan/Emerald City Plan, the University Avenue Specific Plan directly encourages green development through the University Avenue Green Development (UAGD) program.

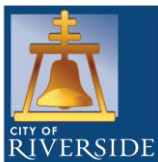
This chapter provides a brief description of some of the current measures aimed at moving the development community toward a more sustainable approach to development. This chapter also introduces the UAGD program, a program aimed at encouraging property owners, developers, architects, landscape architects, building designers and contractors to employ sustainable practices in the construction and operation of projects within the Specific Plan area.

5.2 Current Green Development Measures

CALGreen

The 2010 California Green Building Standards Code, referred to as CALGreen, became effective on January 1, 2011. As the nation's first statewide green building code, CALGreen implements mandatory building regulations for all new construction in the state designed to achieve substantial reductions in greenhouse gas emissions, energy consumption and water use. A key component of CALGreen is a two-tiered system designed to allow local jurisdictions to adopt codes that extend beyond the state mandatory regulations. The tiers are voluntary and designed to become mandatory if, and when, a local jurisdiction chooses to adopt them. As adopted by the City of Riverside, CALGreen achieves substantial reductions in greenhouse gas emissions, energy consumption (exceeding the requirements of Title 24 for energy savings by 20 percent) and water usage without requiring new projects to comply with the requirements of the two-tiered system. However, various incentives are offered through the UAGD Program to projects within the Specific Plan area that implement the voluntary tiers as defined under CALGreen. Section 5.3 of this chapter





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discusses the program in greater detail. CALGreen compliance is inspected and verified by the Building and Safety Division as part of general building code enforcement. To obtain more information on CALGreen, please visit the California Department of General Services' website.

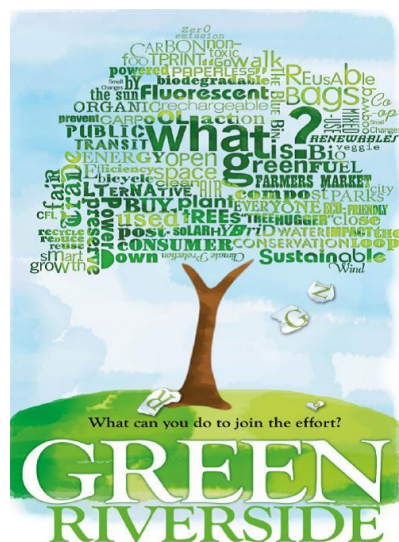
LEED

Developed and administered by the U.S. Green Building Council, the Leadership in Energy and Environmental Design (LEED) green building rating and certification system is designed to promote design and construction practices that increase profitability while reducing the negative environmental impacts of buildings and improving occupant health and well-being. LEED certification provides independent, third-party verification that a building, home or community was designed and built using strategies aimed at achieving high performance in key areas of human and environmental health. The LEED rating system offers four certification levels for new construction – Certified, Silver, Gold and Platinum – that correspond to the number of credits accrued in five green design categories: sustainable sites, water efficiency, energy and atmosphere, materials and resources and indoor environmental quality. In the United States and in a number of other countries around the world, LEED certification is the recognized standard for measuring building sustainability. As with projects that meet the requirements of the voluntary tiers under CALGreen, various incentives are offered to projects within the Specific Plan area that can demonstrate LEED equivalence to the tiers of CALGreen. Section 5.3 of this chapter discusses the UAGD Program in greater detail. To obtain more information on LEED, please visit the U.S. Green Building Council's website.



Green Riverside

In accordance with the Green Action Plan/Emerald City Plan, the City of Riverside offers a multitude of programs designed to encourage residents, property owners, developers and builders to "go green". These include various rebate programs offered by the Riverside Public Utilities (RPU) Department related to construction, solar power, and energy and water efficiency. In addition to rebates, RPU offers financial assistance programs to cover many of the upfront costs of "going green" as well as various tools and tips on how to "go green". To take advantage of all of the programs offered by the City of Riverside, please visit the Green Riverside website.



Riverside Green Builder

Riverside Green Builder (RGB) is a voluntary program primarily for residential production builders. RGB was developed by the City of Riverside and Building Industry Association, Riverside Chapter, with input from home builders and other agencies. A RGB certified home must meet five criteria: Energy Efficiency, Water Conservation, Waste Reduction, Wood Conservation, and Indoor Air Quality. Home designs are the result

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of a flexible performance based approach that allows the home builder to utilize the most cost effective means to meet the program standards. Program incentives include release of electrical meters prior to final inspection, priority field inspection service, guaranteed timelines, overtime inspections and priority electrical design. To obtain more information on RGB, please visit the City of Riverside, Building and Safety Division's website.

5.3 University Avenue Green Development Program

The University Avenue Green Development (UAGD) program is designed to encourage property owners, developers, architects, landscape architects, building designers and contractors to employ sustainable practices in the construction and operation of projects within the Specific Plan area. Similar to RGB, UAGD is a voluntary program that offers various incentives for projects that implement green design measures beyond those already required by law. While RGB is primarily designed for residential production builders, UAGD is offered to multiple types of development. UAGD allows applicants to choose between two options for incorporating green design measures into a project. As outlined below, project applicants can take advantage of the various UAGD program incentives by either meeting the requirements of Tier 1 or Tier 2 under CALGreen or by designing and constructing projects that are LEED equivalent to the tiers of CALGreen.

Option 1 – CALGreen's Two-Tiered System

As mentioned above, a key component of CALGreen is a two-tiered system designed to allow local jurisdictions to adopt codes that extend beyond the state mandatory regulations. UAGD encourages projects to voluntarily implement either Tier 1 or Tier 2 of CALGreen in exchange for the various program incentives outlined in Table 5.3B. Under CALGreen, each tier contains residential and non-residential voluntary measures that are grouped into the following categories:

- Planning and Design
- Energy Efficiency
- Water Efficiency and Conservation
- Material Conservation
- Environmental Quality

Applicants who wish to take advantage of the UAGD program incentives offered for projects that meet the minimum requirements of Tier 1 must satisfy all of the mandatory measures under CALGreen in addition to a set of prerequisite measures within each of the categories listed above and a minimum of one elective measure per category, as defined under CALGreen. Applicants who wish to take advantage of the program incentives offered for projects that meet the minimum requirements of Tier 2 must satisfy all of the mandatory measures under CALGreen in addition to the prerequisite measures of Tier 1 and Tier 2 and three elective



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measures, as defined under CALGreen. Table 5.3A below provides a summary of this system:

Table 5.3A – CALGreen’s Two-Tiered System

CALGreen Tiers
Tier 1
Satisfy all mandatory measures under CALGreen
Satisfy all prerequisite measures under Tier 1
Satisfy a minimum of one elective measure under each category (5 total)
Tier 2
Satisfy all mandatory measures under CALGreen
Satisfy all prerequisite measures under Tier 1 and Tier 2
Satisfy a minimum of three elective measures under each category (15 Total)

In order to receive the selected program incentives, applicants are expected to provide verification that a project meets or exceeds the minimum requirements of Tier 1 or Tier 2. Further, applicants are also expected to satisfy the measures of the chosen tier prior to occupancy of any structures. Please refer to the California Green Building Standards Code for a complete description of the two-tiered system and an interpretation of each measure, available at the California Department of General Services’ website.

Option 2 – LEED Equivalent

UAGD includes an alternative to CALGreen’s two-tiered system for applicants who wish to take advantage of the UAGD program incentives offered to projects within the Specific Plan area. Applicants who design and construct projects that are LEED equivalent to either Tier 1 or Tier 2 of CALGreen are eligible to receive various program incentives, as outlined in Table 5.3B below. Although encouraged, projects do not need to obtain LEED certification in order to take advantage of this option. Similar to CALGreen, LEED measures are grouped into the following categories:

- Sustainable Site
- Energy and Atmosphere
- Water Efficiency
- Material and Resources
- Indoor Environmental Quality

As with Option 1 above, applicants are expected to provide third-party verification that a project is LEED equivalent to the minimum requirements of either Tier 1 or Tier 2 under CALGreen as well as satisfy the all required measures prior to occupancy of any structures. To obtain more information on the LEED rating system and green design measures, please visit U.S. Green Building Council’s website.

Program Incentives

Applicants of projects that meet Option 1 or 2 of the UAGD qualify for a selection of program incentives found in Table 5.3B. Project applicants

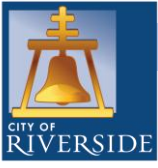
5: DEVELOPING GREEN ON UNIVERSITY AVENUE



must provide evidence to document that a project includes the minimum program measures required to receive the program incentives. Application of the UAGD program to a specific project is subject to review and approval by the City Planner, Community Development Director, or their designee (Approving Authority). Project applicants should work closely with the assigned project planner to ensure proper implementation of the program. Prospective applicants should contact the Planning Division to learn more about the program and how it can benefit projects within the Specific Plan area.

Table 5.3B - Program Incentives

Program Incentives
Development Standards
Reduction in building setback requirements
Increase in building height permitted
Increase in the maximum density permitted
Reduction in the ratio of open space standards
Reduction in the ratio of vehicular parking standards
Entitlements and Permits
Pre-submittal review process
Priority entitlement processing
Priority plan check processing
Priority field inspection services
After-hours plan check (subject to additional fees)
After-hours field inspections (subject to additional fees)
Release of electrical meters prior to final inspection



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SECTION II: LAND USE REGULATIONS AND DEVELOPMENT STANDARDS





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6

Chapter 6: Land Use Regulations, Development Standards and Design Guidelines

6.1 Purpose and Applicability

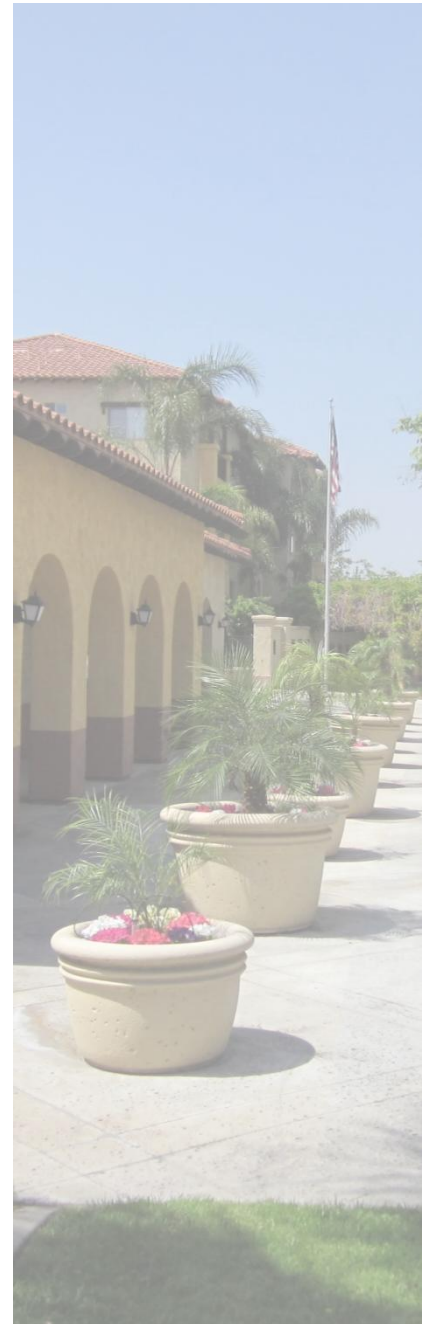
The development and design standards and guidelines for the University Avenue Specific Plan are intended to provide property owners, merchants, developers, and their designers with basic development and design criteria that reinforce the desired building and district character. This chapter was prepared in concert with the Zoning Code and outlines the general provisions and development standards that apply to all properties within the University Avenue Specific Plan area. This chapter also outlines specific development standards and land use regulations for each of the four districts as introduced in Chapter 1 and illustrated in Figure 4.

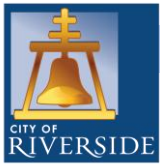
All properties within the Specific Plan area shall be subject to these standards, and property owners, developers, architects, building designers and contractors should use these standards and guidelines in the early design stages of their projects. These standards and guidelines are intended to support the University Avenue Specific Plan objectives and to:

- Provide basic design parameters for all development in the University Avenue Specific Plan area.
- Provide guidance as to the quality and character of individual projects.
- Offer flexibility to accommodate innovative and unique designs.
- Promote design creativity and variation while ensuring consistency in building scale, proportion and pedestrian orientation.
- Create a livable and vibrant environment that compliments the adjacent areas, including the Eastside and University Neighborhoods, the Marketplace and Downtown areas, and the UCR campus.

6.2 Proposed Development Framework

The proposed University Avenue Specific Plan is consistent with the content and structure of the Riverside General Plan 2025, but provides a more specific level of detail for implementation. The objectives and policies stated in the General Plan 2025 are statements that define the City's aspirations and intentions. This chapter is intended to establish the standards required to implement the University Avenue Specific Plan Vision as well as the previously discussed objectives and policies, while acknowledging the opportunities and constraints outlined in Chapter 3.





6.3 University Avenue Specific Plan Districts

In analyzing the University Avenue Specific Plan area it became clear that there were several distinct districts based on common development patterns, existing land uses, historic fabric, and lot sizes, among other factors. As a result, the University Avenue Specific Plan is divided into four districts (as described in Chapter 1 and illustrated in Figure 4) that are unique, not only in terms of land uses and development patterns, but also in terms of the desired land uses, architectural and cultural palette, and densities. For example, the Eastside Neighborhood District, with its tightly-knit neighborhood-serving uses and small-scaled buildings, is much different from the University Village District, with its dense student housing complex and various student-serving uses accompanied by a multi-level parking structure, yet each district retains a sense of cohesiveness within itself. Because of the unique situation in each of the four districts, there are unique standards and visions for each. Sections 6.6 through 6.9 of this chapter describe the vision for each of the four districts and establish the permitted uses and development standards for each district.

Organization of District Sections

Each of the district-specific sections includes the following sections:

District Description

Each section begins with a brief description of the district that describes the key features of the district and briefly outlines the intended look and feel for any future development within the district.

Permitted Uses

The base zone dictates the permitted uses for each individual property within the Specific Plan area (see Figure 8 – Existing Zoning).

Prohibited Uses

The base zone dictates the prohibited uses for each individual property within the Specific Plan area in addition to any of the uses listed in the sections for each district. In cases where the prohibited uses set forth in the Specific Plan are inconsistent with the Zoning Code, the standards of the Specific Plan shall prevail. Any use which is prohibited by state and/or federal law is also strictly prohibited.

Development Standards

Development standards such as density, setbacks, building height, lot size, as well as open space and parking requirements, are laid out in an easy to read table format. Although there may be similar standards between the various districts, the standards are unique to each district based on the several factors such as typical lot size and configuration within the district, existing and adjacent land uses, existing densities and development patterns, and intended or proposed development patterns. Illustrative diagrams also accompany these tables to assist in easy interpretation of the standards. The development standards of the Specific Plan are in addition to all applicable standards found in the Zoning Code. In cases



where the development standards set forth in the Specific Plan are inconsistent with the Zoning Code, the standards of the Specific Plan shall prevail.

Additional Development and Design Standards

Where appropriate, there may be additional development and design standards that specifically apply to properties within the district. For example, all four districts within the University Avenue Specific Plan allow for and encourage mixed-use developments, and as a result there are additional or modified mixed-use development standards that are specific to each district.

6.4 General Mixed-Use Development and Design Standards

To achieve the Vision of University Avenue becoming an active and lively corridor that seamlessly links Downtown, the Marketplace and the UCR campus, the General Plan Land Use designation of the majority of the properties within the Specific Plan area were changed during the General Plan 2025 Program update to Mixed-Use Neighborhood (MU-N), Mixed-Use Village (MU-V), or Mixed-Use Urban (MU-U) (See Figure X). However, in order to develop in accordance with their respective General Plan Land Use designations, these properties will need to be rezoned to the corresponding mixed-use zone as they are developed. To encourage mixed-use development on these properties, it is recommended that the City initiate the necessary zone changes at the time that a viable project is submitted to the Planning Division for design review. As such, the following mixed-use development and design standards shall apply to any mixed-use development throughout the University Avenue Specific Plan unless different than the district-specific standards found within the individual district sections of this chapter, in which case, the latter shall prevail.

A. Site Plan Review Permit and Required Findings

1. New development in any of the four identified districts is subject to a Site Plan Review Permit in accordance with Chapter 19.770 (Site Plan Review Permit) of the Zoning Code. Additionally, any use that requires a Site Plan Review per the Permitted Uses listed in the district sections of this chapter shall also be subject to the standards within 19.770, regardless of whether it is in an existing building. Prior to submittal of a Site Plan Review Permit application, a pre-application conference with the Planning Division staff is encouraged.
2. The Approving Authority may approve a Site Plan Review Permit for new development upon making the following findings:



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- a. The proposed development is consistent with the General Plan 2025, University Avenue Specific Plan and appropriate Neighborhood Plan.
- b. The proposed development, as conditioned, will not have substantial adverse effects on the surrounding property or uses, and will be compatible with the existing and planned land use character of the surrounding area.
- c. The proposed development is appropriate for the site and location by fostering a mixture and variety of land uses within the zone and the general vicinity and contributing to a synergistic relationship between uses.
- d. The proposed development is harmonious with its surrounding environment. Buildings within the proposed mixed-use development project are compatible with each other and are designed as an integrated, unified project. The proposed development must meet the design standards and guidelines of the University Avenue Specific Plan.

B. Phasing

1. For any mixed-use development that is proposed to be constructed in phases, the applicant shall submit a development phasing plan, to be reviewed in conjunction with the Site Plan Review Permit where required, that specifies the chronology of development, including structures, public facilities and infrastructure. The project shall be phased so that supporting public facilities and infrastructure are provided concurrent with their need and are completed before the occupancy of structures. Project phasing may be reviewed independently subsequent to initial approval of the Site Plan Review Permit.
2. If the initial phase of development does not include a mix of uses, the conditions may be applied to the development phasing plan so that a mix of component uses is provided before the completion of the project.

C. Use Regulations

1. Uses in mixed-use zones shall be regulated by the Zoning Code, Chapter 19.150 – Base Zone Permitted Land Use.

D. Development Standards

1. General



Certain development standards may be subject to special conditions. These standards are provided here or as otherwise referenced. Under Site Plan Review, more restrictive development standards may be applied by the City Planning Commission.

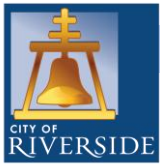
2. Parking Requirements

- a. Parking for uses in the mixed-use districts shall be provided as required in Chapter 19.580 (Parking and Loading) of the Zoning Code. A reduction in the number of required parking spaces may be permitted for mixed-use development and/or stand-alone uses in mixed-use zones subject to the approval of a shared parking arrangement by the approving authority.
- b. Parking spaces shall be specifically designated for non-residential and residential uses by the use of signage, pavement markings and/or physical separation. There should be separate entrances and exits, or a designated lane for residents in order to minimize waiting times for residents.
- c. Parking structures shall be architecturally integrated with the project design and their visual impact minimized through proper siting and design. Parking structures shall include architectural detailing, façade treatment, artwork, landscaping or similar features to enhance the street façade.
- d. Shared driveways and parking arrangements between all uses are strongly encouraged.
- e. Parking between the street-adjacent sidewalk and buildings shall be prohibited.

3. Special Provisions for Live/Work Units

The following provisions apply to live/work units, where permitted:

- a. *Floor area requirements:* The minimum floor area of a live/work unit shall be 750-square-feet.
- b. *Access to units:* Access to individual units shall be from common access areas, corridors or hallways.
- c. *Internal layout:* All living space within the live/work unit shall be contiguous with, and an integral part



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of, the working space, with direct access between the two areas.

- d. *Occupancy and employees:* At least one of the full-time workers of the live/work unit shall reside in the unit. The residential area shall not be rented separately from the working space. The business activity occupying the live/work unit may utilize employees in addition to residents as necessary.
- e. *Retail sales:* Retail space may be integrated with working space.
- f. *Business Tax Certificate:* A business tax certificate shall be obtained in compliance with the Municipal Code, Title 5, for business activities conducted within the live/work unit.

F. Design Standards and Guidelines

The purpose of this section is to facilitate high quality mixed-use development within the four districts of the University Avenue Specific Plan. Innovative project design, particularly involving infill development is required. These standards and guidelines address site planning and building design, and are in addition to the development standards above, as well as the Citywide Design and Sign Guidelines.

1. Setbacks

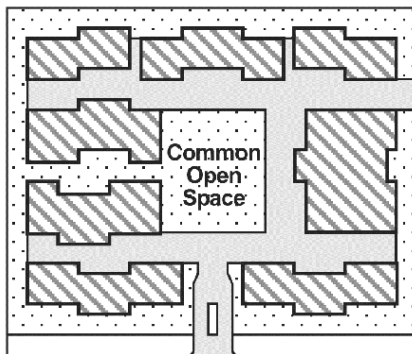
- a. The front setback area shall include a hard-surface expansion of the sidewalk in combination with decorative hardscaping and pockets of dense landscaping. Walkway connections to building entrances shall include special paving treatment or materials. The use of awnings, canopies and arcades shall be provided as appropriate to provide visual interest and shade.
- b. The front setback area shall be reserved for outdoor use, such as outdoor patio dining, display, public art, entry forecourts, or other amenity appropriate to an urban development.

2. Building Siting, Orientation and Entrances

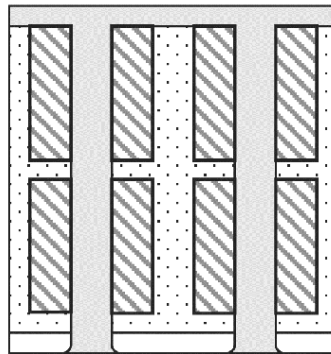
- a. Buildings should be sited to avoid random and irregular building relationships, and shall be arranged to create a sense of unity and overall harmony. To the maximum extent possible, new structures shall be clustered to create plazas and pedestrian malls and avoid the creation of



"barrack-like" rows of structures. Where clustering is impractical, a visual link between separate structures should be established. This link can be accomplished through the use of an arcade system, trellis or other open structure (See Diagram 1 – Open Space).



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Orient buildings to create useable open space in a convenient location.

Diagram 1 – Open Space

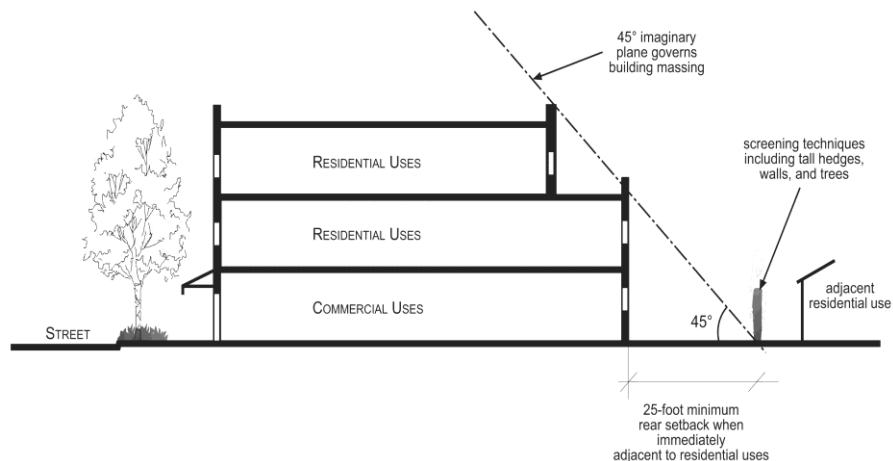
- b. The main entrance or entrances shall be oriented to the street or major plazas or open space. Main entries to buildings should be clearly demarcated, visible and accessible from the street and/or pedestrian walkways. Secondary entries may be from parking areas.
 - c. Commercial facilities in mixed-use projects should be oriented to the street, with parking generally located in the rear of buildings. The perimeter of parking areas and driveways adjacent to streets and sidewalks shall be screened with an attractive low wall, berm, fence or landscaping.
 3. **Scale and Mass**
 - a. The scale and mass of a new mixed-use development should be consistent with neighboring developments and not overwhelm them with disproportionate size or incompatible design. Buildings shall step down to lower-profile buildings on adjacent properties.



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- b. At residential edges, buildings should maintain low profiles to provide a transition between urban and residential areas (Diagram 2 – Scale and Mass). Taller elements of the building shall increasingly step back from adjacent single-family residential zones. No portion of the building, excluding parapets, shall be above an imaginary plane drawn at the rear property line that is adjacent to a residential zone, and extended at an angle of 45 degrees toward the center of the property.



Setbacks and massing for buildings adjacent to residential uses.

Diagram 2: Scale and Mass

4. Building Modulation and Articulation

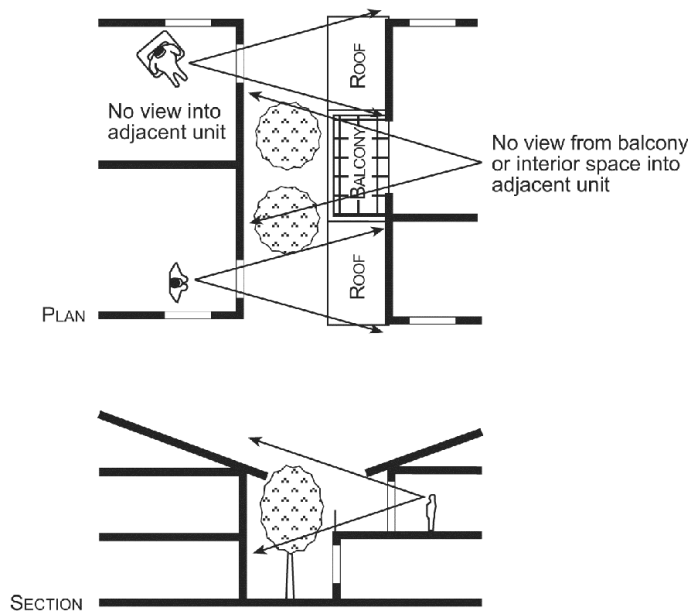
- a. Building design shall avoid large monotonous facades, long straight-line building fronts, plain box shapes, and barren exterior treatment. All building elevations visible from a public way, including freeways, shall be highly articulated, completely screen all roof equipment and incorporate the chosen design theme in a consistent manner.
- b. Commercial facades of mixed-use projects should be modulated at least every 50 feet by changes in building mass or facade treatment, such as projected entrance windows, roof form or other architectural features.
- c. Building facades shall be designed so as to give individual identity to each vertical module of residential units, using techniques such as providing a deep notch between the modules; varying architectural elements between units (e.g.,



window color, roof shape, window shape, stoop detail, railing type); providing porches and balconies; varying color or materials of each individual module within a harmonious palette of colors and materials, etc.

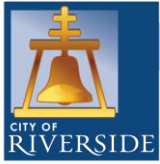
Privacy for Residential Units

- d. Buildings shall be oriented to promote privacy to the greatest extent possible. In mixed-use projects, residential windows should face away from loading areas and docks. To the extent residential windows face the windows of an adjacent unit, the windows should be offset to maximize privacy.
- e. Windows, balconies or similar openings should be oriented so as not to have a direct line-of-sight into adjacent units within the development (Diagram 3 – Privacy for Residential Units). In addition, units above the first story should be designed so that they do not look directly onto private patios or backyards of adjoining residential property or units.
- f. Landscaping may be used to aid in privacy screening and as a buffer from commercial development.



Plant appropriate trees and offset windows and balconies (or patios) to maintain privacy between residential units.

Diagram 3: Privacy for Residential Units



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5. Vehicle Circulation and Access

- a. Site access and internal circulation shall promote safety, efficiency and convenience. Vehicular traffic shall be adequately separated from pedestrian circulation. Vehicular entrances shall be clearly identified and easily accessible to minimize pedestrian/vehicle conflict.
- b. The number of site access points or driveway aprons shall be minimized for aesthetic purposes, to achieve efficient and productive use of paved access ways and to eliminate traffic and pedestrian hazards. They should be located as far as possible from street intersections, and should be coordinated with existing or planned median openings and driveways on the opposite side of the roadway. Common driveways that provide vehicular access to more than one site are encouraged.

6. Pedestrian Circulation

- a. All new uses shall be oriented and designed to enhance pedestrian movement to and between adjacent uses.
- b. New development shall include pedestrian walkways that shall be separated from vehicular traffic to the extent possible. Pedestrian entrances and walkways shall be clearly identified and easily accessible to minimize pedestrian/vehicle conflict.
- c. Pedestrian walkways shall link dwelling units with commercial facilities in the project, common open space, plazas and courtyards, parking areas and public sidewalks.
- d. Decorative paving or some other method shall be used to delineate crossings at circulation drives and parking aisles.

7. Plazas and Courtyards

New development shall incorporate outdoor plazas and courtyards into their design. Buildings may be clustered to create usable pedestrian areas.

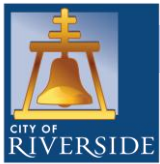
8. Open Space

- a. Common open space areas shall be convenient to the majority of dwellings, and shall contain



amenities appropriate to the project's size (Diagram 1 – Open Space).

- b. Private useable open space shall be contiguous to the unit served and screened from public view for privacy. All balconies and patios that front a public street shall be substantially enclosed for screening and privacy.
 - c. Rooftop open space may be used as common useable open space. It may also be used as private useable open space, when directly accessible to the unit(s) it serves.
9. **Outdoor Display and Storage**
Commercial outdoor display and storage shall not be permitted except as specified in Chapters 19.500 (Outdoor Display of Incidental Plant Materials), 19.505 (Outdoor Display and Sales) and 19.510 (Outdoor Storage) of the Zoning Code (Title 19).
10. **Trash Receptacles and Enclosures**
 - a. The residential units shall maintain a trash storage container area that is separate from that used by the commercial uses. It shall be clearly marked for residential use only.
 - b. All trash storage areas for commercial uses shall be located so as to be convenient to the commercial users and where associated odors and noise will not adversely impact the residential uses.
 - c. The provisions of Chapter 19.554 of the Zoning Code (Trash/Recyclable Materials Collection Area Enclosures) regarding requirements for the screening of trash receptacles shall apply.
11. **Mechanical Equipment Screening**
The provisions of Chapter 19.555 of the Zoning Code (Outdoor Equipment Screening) regarding required screening of mechanical equipment shall apply.
12. **Bicycle Parking and Storage Areas**
Residential mixed-use developments shall provide common bicycle storage areas for the residents as follows: two (2) bicycle storage units for every five (5) dwelling units for the first 20 dwelling units, and one (1) bicycle storage unit for every five (5) additional dwelling units.



G. Performance Standards

The purpose of this section is to ensure that residential uses in mixed-use developments are not adversely impacted by adjacent commercial uses, including but not limited to traffic, noise, light and safety impacts. In the interests of both the residents and the businesses, no Site Plan Review Permit shall be approved for a project unless the project is designed to meet the following performance standards, in addition to performance standards set forth in Chapter 19.590 of the Zoning Code (Performance Standards).

1. Noise

- a. Residential units shall be constructed and designed to meet the performance standards in Title 7 (Noise Control) and Title 16 (Buildings and Construction). Proper design may include, but shall not be limited to, building orientation, double windows, wall and ceiling insulation and orientation of vents.
- b. Commercial uses shall be designed and operated, and hours of operation limited where appropriate, so that neighboring residents are not exposed to offensive noise, especially from traffic, routine deliveries or late-night activity. No amplified sound, including music, shall be audible to neighboring residents.
- c. Common walls between residential and non-residential uses shall be constructed to minimize the transmission of noise and vibration.

2. Security

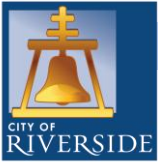
- a. The residential units shall be designed to ensure the security of residents through the provision of separate and secured entrances and exits that are directly accessible to secured parking areas. Where residential units are in the same structure as a commercial use, access to residential units shall be from a secured area located on the first floor at the ground level.
- b. Nonresidential and residential uses located on the same floor shall not have common entrance hallways or common balconies.
- c. Any multi-family residential development or group home shall participate in the City's Crime Free



Multi-Housing Program, or successor equivalent program.

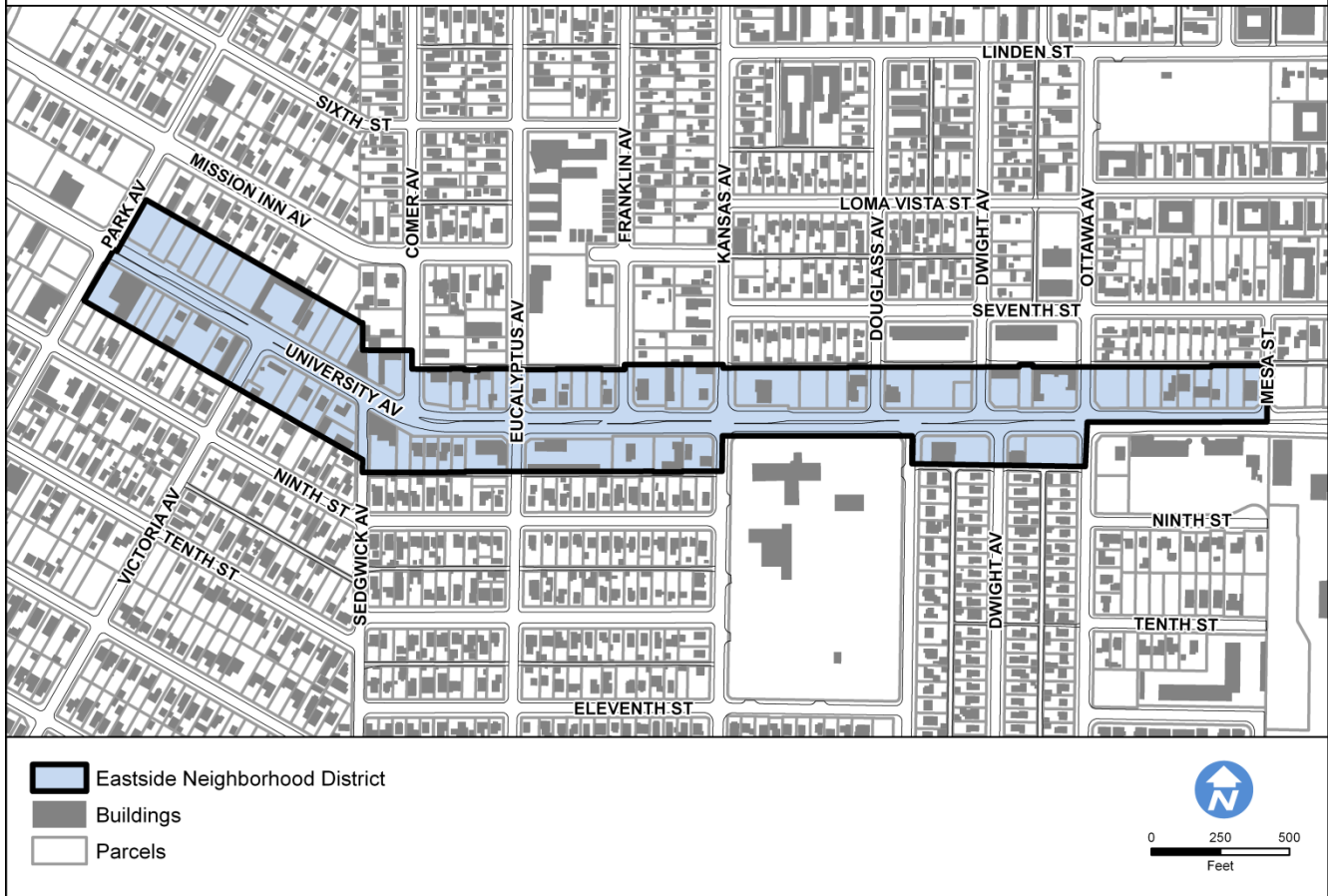
3. Light and Glare

- a. All outdoor lighting associated with commercial uses adjacent to or within the immediate vicinity of residential uses shall be designated with fixtures and poles that illuminate commercial uses, while minimizing light trespass into residential areas.
- b. The candlepower of outdoor lighting shall be the minimum required for safety purposes.
- c. Energy-efficient ENERGY STAR® certified lighting fixtures and equipment should be used. Energy-efficient means of lighting, including light sensors, low voltage lighting, fiber optics and solar lighting should be used where applicable. Timers or other controls should be used to assure that lights are on only when needed. Use light-colored surface material where additional light is needed to take advantage of higher reflectance values.
- d. The provisions of Section 19.590.070 of the Zoning Code (Light and Glare) shall apply.
- e. The provisions of Chapter 19.556 of the Zoning Code (Lighting) shall apply.



6.5 Eastside Neighborhood District

Figure 17
Eastside Neighborhood District



District Description

To many, the University Avenue corridor represents the hub or activity center of the Eastside Neighborhood, a diverse and vibrant community primarily composed of single-family residential areas with some multiple family residential areas that lies in the heart of the eastern portion of the City of Riverside. The Eastside Neighborhood District, encompassing roughly 34 acres within the Eastside Neighborhood, includes the majority of the properties fronting on University Avenue, between Park Avenue and Mesa Street (See Figure 17) and features a mix of neighborhood-serving commercial, restaurant, office, and other uses, as well as several long-established motels. Although not within the boundaries of the Specific Plan, the centerpiece of the University Avenue corridor in this District is the Caesar Chavez Community Center and Bobby Bonds Sports Complex, which occupy the building and site of the former University Heights Junior High School built in 1928 and listed on the National Register of Historic Places. Overall, the District is comprised of mostly small-scaled, one-story buildings on shallow lots (most lots measuring less than 150 feet in depth) that abut single-family residences or access alleys along the rear of the properties.



Due to recent Redevelopment efforts, many neglected properties and properties developed with undesirable uses within this District have been acquired, razed and/or consolidated. This has resulted in several vacant opportunity sites ripe for infill development. This District also contains several historic structures that are prime for adaptive reuse, including a former fire station designed by G. Stanley Wilson. Further, a major streetscape and median enhancement program was recently completed along the University Avenue corridor. The highlight of this project was the construction of a raised landscaped median along the entire corridor where a left turn lane had previously run the length of the Avenue.

The Eastside Neighborhood District seeks to preserve and enhance all of these components while allowing for growth and development of underutilized properties in a manner that is sensitive to existing historic properties and surrounding single-family residential uses. Future development in this District should be small-scaled, featuring wide sidewalks and shared parking in the rear with buildings of no more than two stories that have a strong street presence. Front setback areas should feature a hard-surface expansion of the sidewalk in combination with decorative hardscaping and pockets of dense landscaping to create attractive and useable outdoor gathering spaces that encourage indoor activities to relocate to the outside while drawing pedestrian traffic on-site.

Eastside Neighborhood District Illustrated

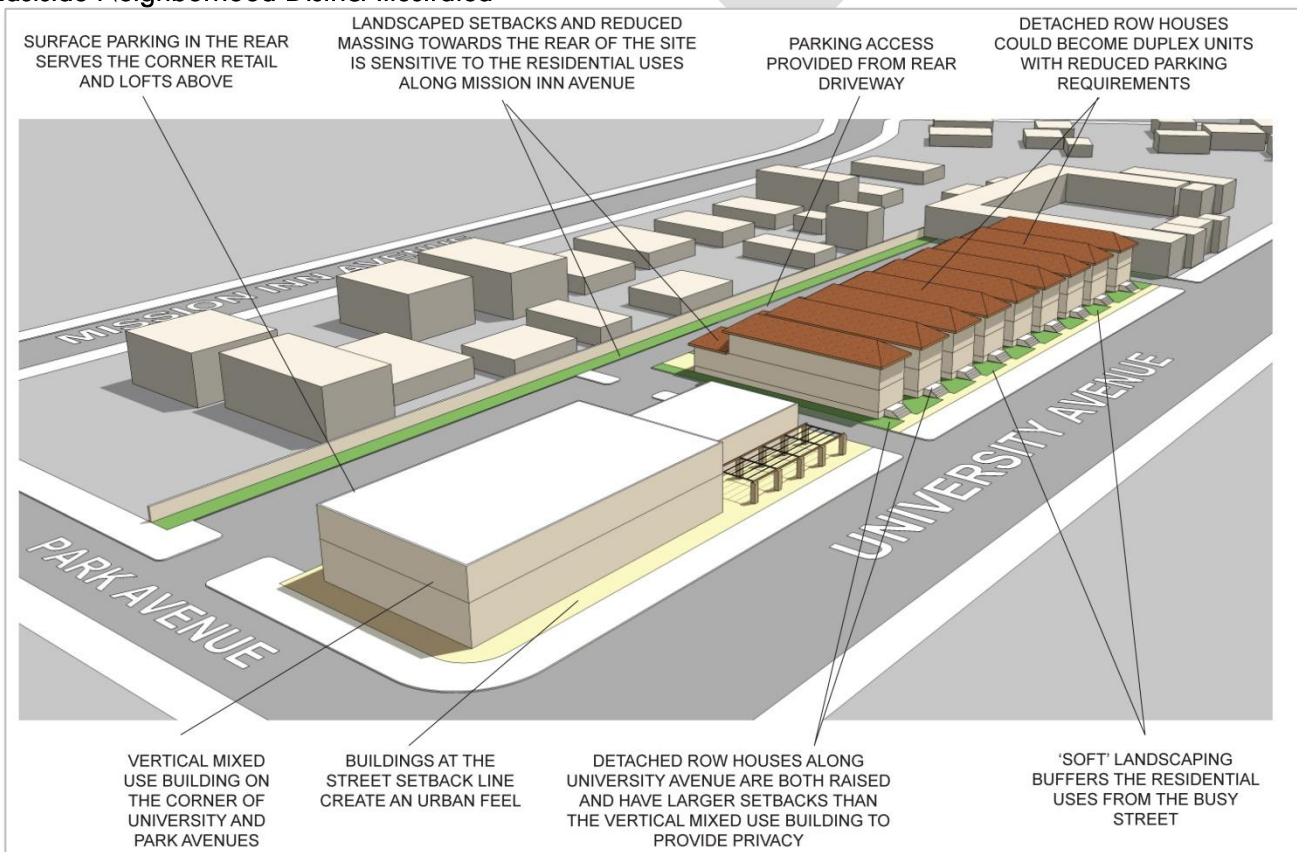


Image 6.5-A: This image illustrates how a vertically mixed-use building, retail uses and residential uses can be developed on this gateway site in the Eastside Neighborhood District. The project illustrates increased setbacks from, as well as reduced massing towards, the single-family residential neighborhood along Mission Inn Avenue. Parking is distributed between surface parking in the rear of the site for the vertical mixed-use building and in individual garages for the row houses. The row houses could become duplex flats if the development standards for this site allow reduced or tandem parking for these units. The setback along the ground floor retail uses is shown to have “hard” landscaping while the setback in front of the residential uses is shown to have more “soft” landscaping. The landscaping is shown as green and further detailed in Image 6.5-B.

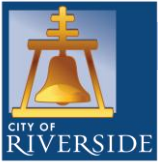


Image 6.5-B: This image illustrates a vertically mixed-use building with ground floor retail uses and urban lofts on the upper floor. An active ground floor retail presence activates University Avenue at this gateway site. The variation in massing, setbacks, building form, and high-quality materials and finishes provides an interesting street presence and urban feel. Canopies and trellises provide areas for outdoor dining, further adding to the street activity. The upper story lofts have a separate and clearly identifiable entrance for access from the street. Additional access points are provided from the surface parking lot in the rear of the site. Landscaping in pots and at-grade planters help soften the experience of the setback areas. The intersection treatment with enhanced paving and median trees is also visible in this illustration.

Permitted Uses

Within the Eastside Neighborhood District, the base zone shall dictate the permitted uses for each individual property (see Figure 8) unless specifically prohibited in the *Prohibited Uses* Section below. Base zone permitted uses are found in the Permitted Uses Table, Section 19.150.020(A) of the Zoning Code. All permitted uses are subject to any necessary permits as listed below or in the Zoning Code as well as all other requirements of the Specific Plan and Zoning Code, as applicable. Incidental uses shall be subject to the Incidental Uses Table found in Section 19.150.020(B) of the Zoning Code.

Rezoning for Mixed-Use

As discussed earlier in this chapter, the General Plan Land Use designation of the majority of the properties within the Specific Plan area were change during the General Plan 2025 Program update to Mixed-Use – Neighborhood (MU-N), Mixed-Use – Village (MU-V), or Mixed-Use – Urban (MU-U) (See Figure 7). The Land Use designations for all of properties within the Eastside Neighborhood District were changed to Mixed-Use, with the properties between Park Avenue and Ottawa Avenue designated MU-N and the properties between Ottawa Avenue and Mesa Street designated MU-V. While the current base zone of each individual property within the Specific Plan area dictates the standards for land use and development, it is strongly



recommended that properties be rezoned to the corresponding mixed-use zone per the Zoning Code and in accordance with their respective General Plan Land Use designations, as they are developed.

To encourage mixed use development on these properties, it is recommended that the City initiate the necessary zone changes at the time that a viable project is submitted to the Planning Division for design review. For properties that adopted the respective Mixed-use zone in accordance with their General Plan Land Use Designation, the permitted uses found under the mixed-use zones in the Permitted Uses Table (Section 19.150.020(A)) of the Zoning Code shall apply.

Prohibited Uses

Within the Eastside Neighborhood District, the base zone shall dictate the prohibited uses for each individual property in addition to the uses listed in Table 6.5A – Eastside Neighborhood District Prohibited Uses. In cases where the prohibited uses set forth in the Specific Plan are inconsistent with the Zoning Code, the standards of the Specific Plan shall prevail. Base zone prohibited uses are found in the Permitted Uses Table, Section 19.150.020(A) of the Zoning Code. Any use which is prohibited by state and/or federal law is also strictly prohibited. The uses listed in Table 6.5A are strictly prohibited within the Eastside Neighborhood District.

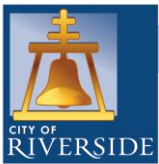
Table 6.5A – Eastside Neighborhood District Prohibited Uses

Alcohol Sales – Off-sale ¹
Bail Bonds Offices
Drive-thru Businesses
Group Housing (Convalescent Homes, SRO's, Alcohol & Drug Treatment Facilities, more than 6 Occupants)
Home Improvement, Sales and Service – Retail: 20,000 square feet or More
Homeless Shelters
Hotels and Motels
Pawn Shops
Recycling Facilities
Student Housing
Vehicle Fuel Stations
Vehicle Parts and Accessories
Vehicle Repair Facilities
Vehicle Sales, Rental and Leasing – New and Used
Vehicle Wash Facilities

¹Except as an incidental use to florist shops and establishments that do not propose to sell alcohol as their principal business and contain 15,000 square feet or more of gross floor area. Subject to the Chapter 19.450, Alcohol Sales, of the Zoning Code.

Development Standards

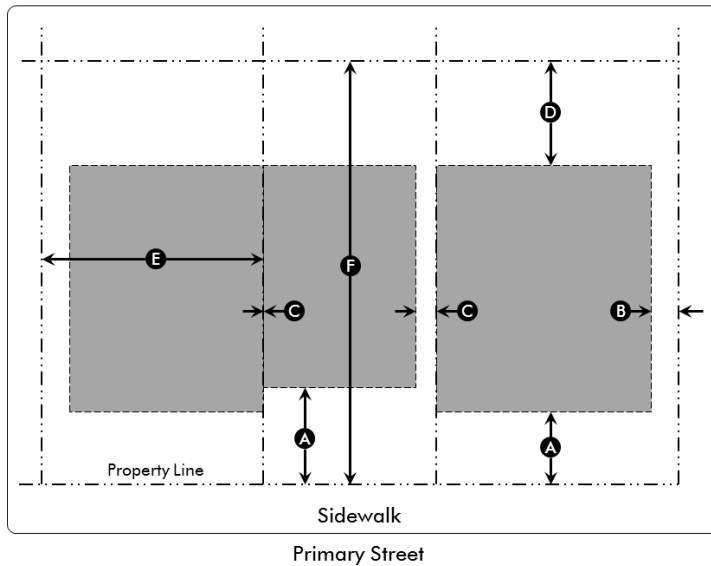
Within the Eastside Neighborhood District, the standards listed in Table 6.5B - Eastside Neighborhood District Development Standards shall dictate the development standards for each individual property, in addition to all applicable standards found in the Zoning Code. In cases where the development standards set forth in the Specific Plan are inconsistent with the Zoning Code, the standards of the Specific Plan shall prevail.



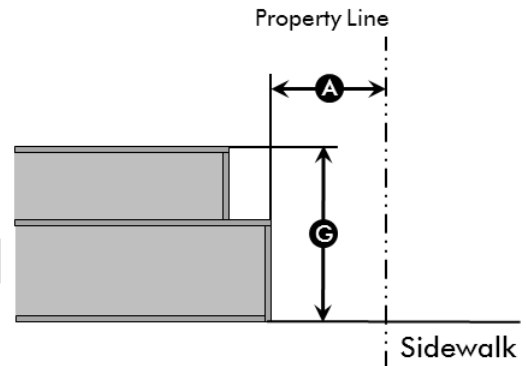
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Table 6.5B Eastside Neighborhood District Development Standards



Side Street



Building Placement		
Setback		
Front ^{1,2}	A	10' – 15'
Side Street	B	5' Minimum
Interior Side	C	0' Minimum
Rear	D	15' Minimum
Minimum Lot Standards		
Lot Area		Base Zone
Lot Width	E	Base Zone
Lot Depth	F	Base Zone
Building Height		
Maximum Height	G	35'/2 Stories
Standards for Front Setback Area ³		
Surface Coverage ⁴		
Hardscaping		50% – 85%
Landscaping		15% – 50%
Sidewalk Widening ⁵		3' – 6'
Additional Standards for Residential as Part of Mixed-Use Development		
Maximum Density		
With less than one acre lot size		10 du/acre
With one to two acres min. lot size		15 du/acre
With more than two acres min. lot size		20 du/acre
Open Space ⁶		
A. Minimum Private Open Space ⁷		50 sq.ft./du for at least 50% of the units
B. Minimum Common Open Space ⁸		50 sq.ft./du

Parking

Mixed-Use

1. Parking standards for mixed-use developments shall comply with the Zoning Code, Chapter 19.120.060.B. A reduction in the number of required parking spaces may be permitted for mixed-use developments and/or stand-alone uses in mixed-use zones subject to the approval of a shared parking arrangement.
2. See Section 6.4 (General Mixed-Use Development and Design Standards) of this Chapter for additional parking standards.

Base Zone

Parking standards shall comply with the Zoning Code, Chapter 19.580.

¹See the *Additional Development and Design Standards for the Eastside Neighborhood District* Section for additional requirements.

²Applies to building footprint only. 2nd floor can be stepped back to allow for balconies, building articulation and other design features.

³Applies to setbacks along University Avenue only.

⁴The entire front setback area, including driveways, shall include a mix of decorative hardscaping and landscaping.

⁵The existing sidewalk shall be widened by an additional 3 to 6 feet with decorative hardscaping along the entire street frontage.

⁶Can be a combination of indoor and outdoor open space for mixed-use developments. Includes patios and balconies.

⁷Private usable space shall have a minimum dimension on any side of 5-feet. Private usable open space can also be met through equivalent design features as approved by the Planning Commission.

⁸Common usable open space may be divided into more than one area; however, each area shall be a minimum of 625-square feet, with no dimension on any side of less than 25-feet.



Additional Development and Design Standards for the Eastside Neighborhood District

In accordance with the developments standards in Table 6.5B, the following development and design standards apply to all properties within the Eastside Neighborhood District.

Front Setback Area (See Figure 18)

1. The pedestrian path of travel (e.g. sidewalks) along the street frontage shall be widened by 3 feet to 6 feet (in accordance with the development standards above) through the use of decorative concrete pavers and/or other decorative hard-surface materials to accommodate higher levels of pedestrian foot traffic.
2. A combination of decorative hardscaping and pockets of dense landscaping shall be used along the entire front setback area (including driveways) to create attractive and useable outdoor gathering spaces that encourage indoor activities to relocate to the outside while drawing pedestrian traffic on-site.
3. Hardscaping materials shall include decorative concrete pavers and/or other decorative hard-surface materials that complement the building(s) on site.
4. Landscaping materials shall include dense, yet drought-tolerant, vegetation arranged in pockets, strategically placed to maximize the usable area within the front setback.
5. Landscaping materials shall include large specimens that provide shade and, at the same time, maintain a clear view of building signs from the street frontage.
6. A combination of raised and ground planters as well as decorative pot containers shall be used for all landscaping areas.
7. Tree grates shall be provided for all trees within the sidewalk adjacent to the front setback area to maximize the pedestrian path of travel.
8. Within the front setback area, a minimum of two outdoor amenities listed in Table 6.6C shall be provided:

Table 6.5C – Front Setback Outdoor Amenities

Outdoor Dining	Shade Structures
Water Features	Artistic Bicycle Racks
Art Display Areas	Pave Art
Outdoor Seating	Other Amenities (As deemed acceptable by the Approving Authority)

Figure 18
Eastside Neighborhood Front Setback Treatment

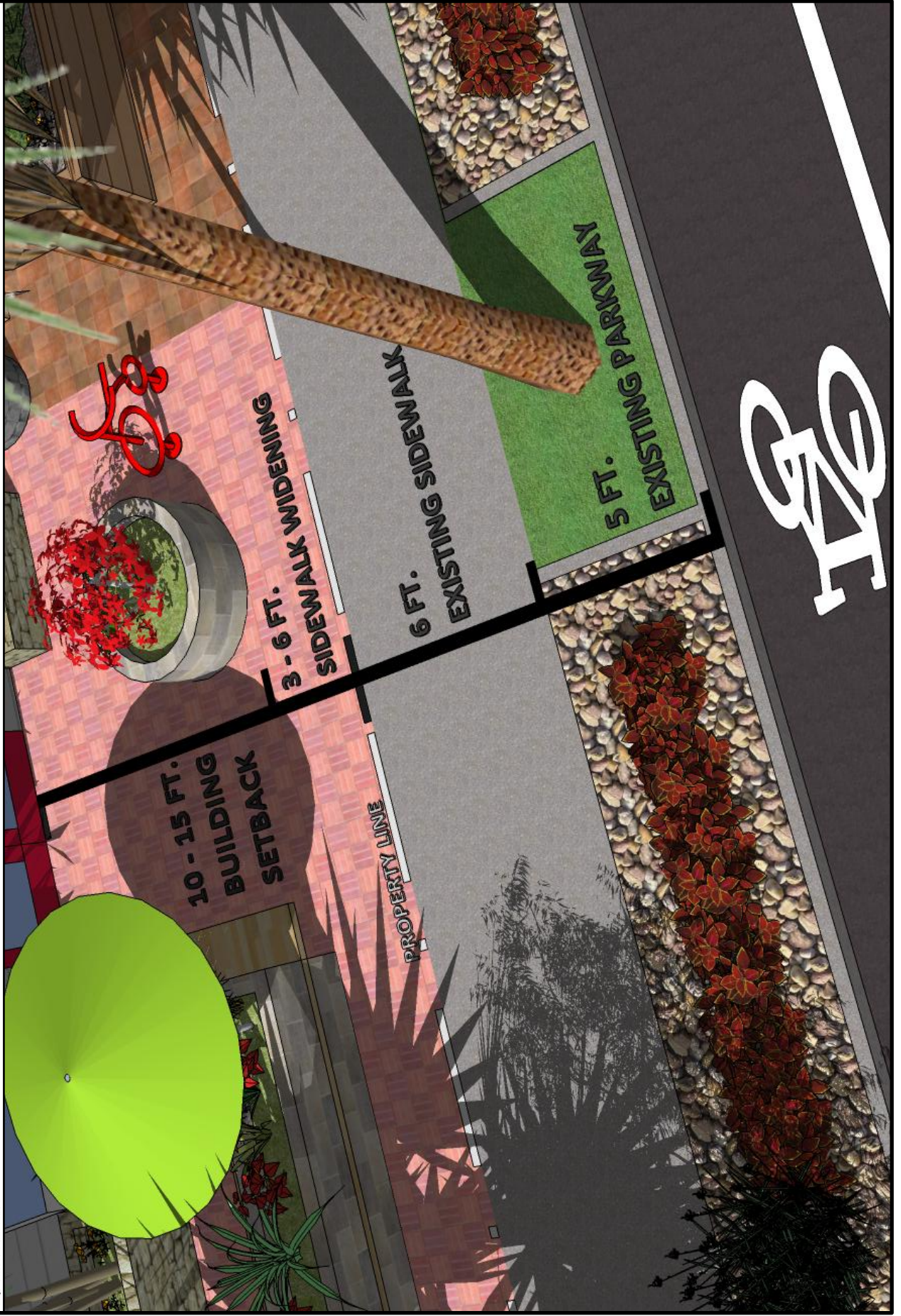




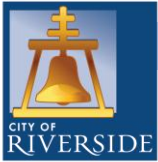
Image 6.5-C: This image illustrates the desired scale, massing, and variation for new development within the Eastside Neighborhood District. The use of varied colors and materials, well-articulated building volumes and suitable mix of hardscaping and landscaping all contribute to providing an appropriate urban scale that is responsive to its surroundings.



Image 6.5-D: This image illustrates the appropriate treatment of front setback areas. A good balance of outdoor amenities, including bicycle racks, art display areas, sidewalk dining, shade and seating areas encourages indoor activities to relocate to the outside while inviting pedestrians on-site.



Image 6.5-E: This image illustrates the advantage of widening the sidewalk. By providing a few feet of decorative and unobstructed hardscaping along the entire frontage of existing properties, the pedestrian path of travel is enhanced and instantly connected to the amenities and activities within the front setback areas.



Building Entrances and Orientation

9. Buildings shall consist of building materials, colors and detailing to fit with the character of the existing surrounding neighborhood.
10. Primary building entrances should be easy to identify along University Avenue.
11. Buildings should be oriented to a defined pedestrian walkway or street.
12. Building materials and colors should exhibit quality and help establish a human scale while providing visual interest.
13. To facilitate the creation of a sense of place, developers are encouraged to incorporate artists into the design team from the inception of planning in order to integrate art into their projects.
14. Special attention should be given to the design of project and building corners as an opportunity to create visual interest and invite activity.
15. Larger sites should feature places where people can gather. Public spaces should contribute to the overall sense of place and site identity and help to attract pedestrian users to the development.
16. Loading docks should be located in areas that have the least amount of impact on residential uses.

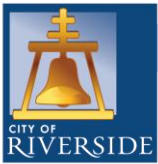
Building Elevations

17. Ground floor facades of buildings incorporating retail uses should be differentiated from upper stories, allowing for flexibility and change of retail tenants. Retail features, which make up the ground floor façade shall include: retail storefronts; awnings and canopies; sign bands; and cornices. Retail storefronts should be at least 75% glazing and the glazing should be non-tinted and unreflective.
18. New infill should be planned and designed in a manner that protects, sustains, and enhances the existing architectural character of University Avenue.
19. In addition to the setback requirements established, the new infill should match the existing building's width, height and massing.
20. New infill buildings should maintain the size and rhythm of space between buildings along the established streetscape.
21. New mixed-use infill development should conform to the proportion, scale and massing of existing residential or commercial buildings adjacent to the infill development.
22. Larger architectural structures can achieve visual harmony with smaller buildings by the use of repetitive massing, roof forms, materials used and cladding color. New, larger buildings may also be subdivided into smaller elements reflective of the mass or detailing of surrounding buildings (i.e. roof form may employ gables, mansard windows, front porches, colonnades, etc.)

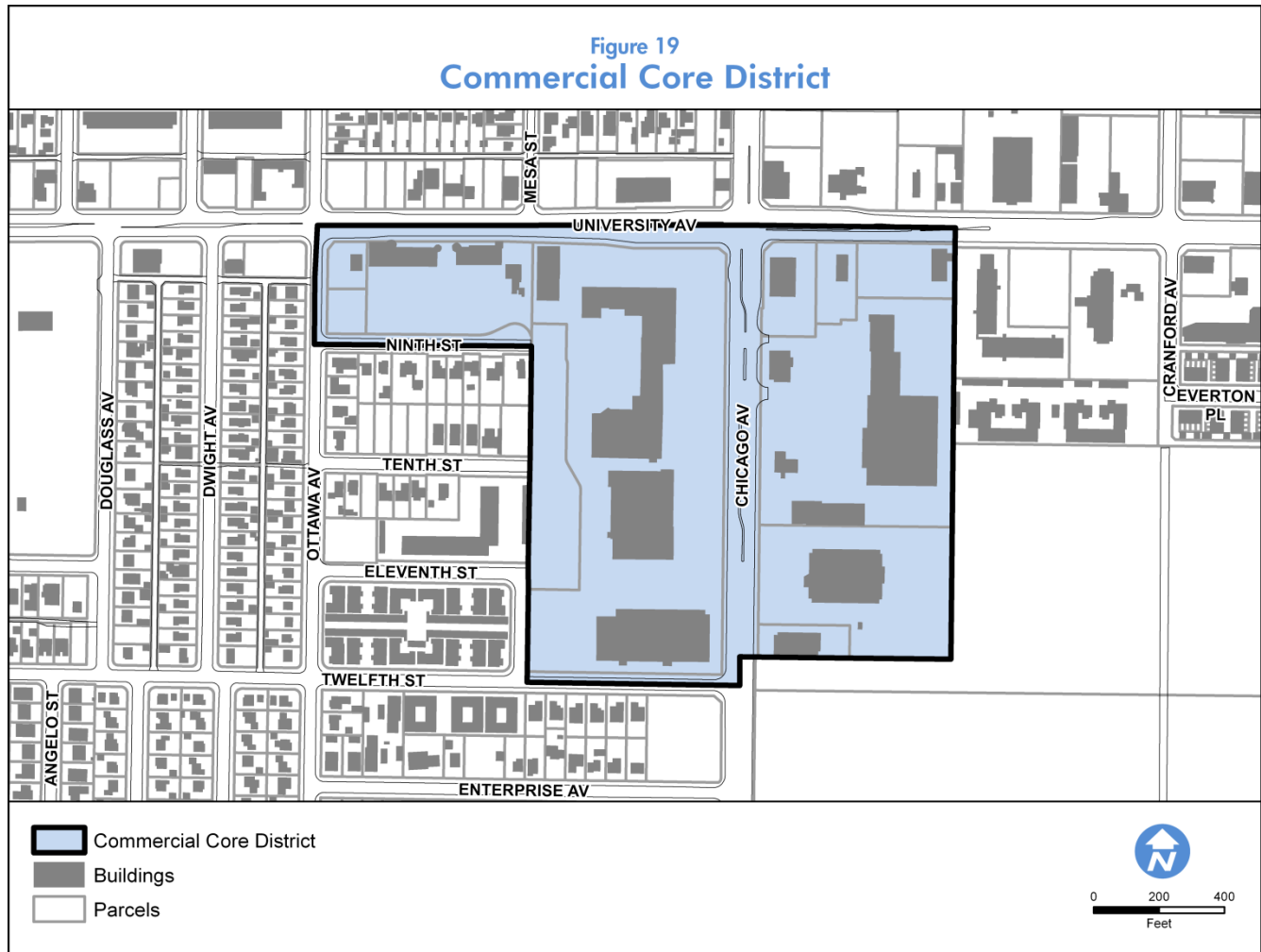


23. The entrances to mixed-use buildings should always be oriented to and be highly visible from the street. Entrances for retail uses must be separated from residential entrances. A well-defined separation between different uses in a mixed-use commercial building is important in order to maintain visual clarity. Additional residential or service entrances connected to the rear parking may be provided from the rear or side of the buildings.
24. Materials and textures applied to the front façade should continue around the building.
25. New buildings and conversions of the existing residential buildings into commercial use should complement the basic façade elements, which include a transparent glazed lower storefront, a sign fascia over a projecting cornice and a solid upper façade punctuated by vertical windows.

DRAFT



6.6 Commercial Core District



District Description

Bounded by University Avenue to the north, Twelfth Street and the UCR campus to the south, Ottawa Avenue and single and multi-family residential uses to the west, and a hotel and multi-family residential uses to the east, the Commercial Core District is located in both the Eastside and University Neighborhoods. As the geographic center of the University Avenue Specific Plan area, this 46-acre District contains many of the broader-appeal uses and amenities that serve the surrounding Eastside and University Neighborhoods as well as the UCR community.

The Town Square Shopping Center comprises the largest single parcel (15 acres) within this District and contains a large mix of commercial retail uses, including a pharmacy, apparel stores, restaurants, personal services, a grocery store and an indoor shopping center. Straddling the westerly edge of the shopping center is a vacant 3.5-acre site that is prime for development of a large multi-family residential project. In fact, the site has been identified as a potential infill site under the recent General Plan 2025 Housing Element update. The highly popular Eastside Library and Cybrary, a recently constructed community library that is part of the City of Riverside's public library system, is also located within the Town Square Shopping Center. The newly

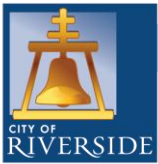


constructed United Education Institute (UEI) building is located on the westerly side of the Commercial Core District, along University Avenue.

On the easterly side of the District lies the Chicago Center Shopping Area, another large shopping center comprised of four parcels totaling 14.7 acres. Like the Town Square Shopping Center located on the opposite side of Chicago Avenue, this shopping center contains a similar mix of commercial retail uses, including drive-thru restaurants, apparel stores, two banks, and a large grocery store. The southeasterly corner of this District is comprised of two parcels. The larger of the two parcels contains a branch office of the United States Postal Service while the other contains a small one-story commercial building that is currently vacant. The previous University Avenue Specific Plan area excluded the two subject parcels due to the fact that the parcels did not front on University Avenue. However, in recent years it has become apparent that the parcels have a strong connection to activities along the University Avenue corridor as well as the surrounding community. Further, with the recent update to the UCR Long Range Development Plan (LRDP) and the anticipated School of Medicine, proposed to be located in close proximity, it is logical to include the subject parcels as part of the Specific Plan area.

The Commercial Core District has benefited greatly in recent years from several projects. Thanks to the Façade Improvement Program (administered through the former Redevelopment Agency) the Town Square Shopping Center underwent a major façade renovation that gave the entire shopping center a much needed makeover. The recent addition of the Eastside Library and Cybrary has also been an immense asset for the surrounding community and has brought an increase in foot traffic to the Town Square Shopping Center. Further, the new high-quality UEI project, which placed the building closer to the street and parking in the rear, has made it a prominent fixture along the University Avenue corridor. Finally, a major streetscape and median enhancement program was recently completed along the entire University Avenue corridor.

The Commercial Core District should continue to allow larger-scale projects with integrated residential, office and retail uses that foster new businesses with a broader appeal. Improved pedestrian connectivity between the various components of this District is critical. The future multi-family residential project on the westerly edge of the Town Square Shopping Center should seamlessly connect the EUI site with the shopping center through a series of safe and well designed pedestrian walkways. Similarly, pedestrian connectivity across Chicago Avenue should be improved to better connect the Town Square Shopping Center with the Chicago Center Shopping Area. Large-scale projects to reconfigure the shopping centers should be designed with buildings pushed closer to the street to allow for large pedestrian gathering spaces (such as plazas) to be located in the interior of the site. Of equal importance is the pedestrian connectivity along the University Avenue corridor. As is the case with all street frontages within the Specific Plan area, the front setback areas in this District should feature a hard-surface expansion of the sidewalk in combination with decorative hardscaping and pockets of dense landscaping to create attractive and useable outdoor gathering spaces that encourage indoor activities to relocate to the outside while drawing pedestrian traffic on-site.



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Commercial Core District Illustrated

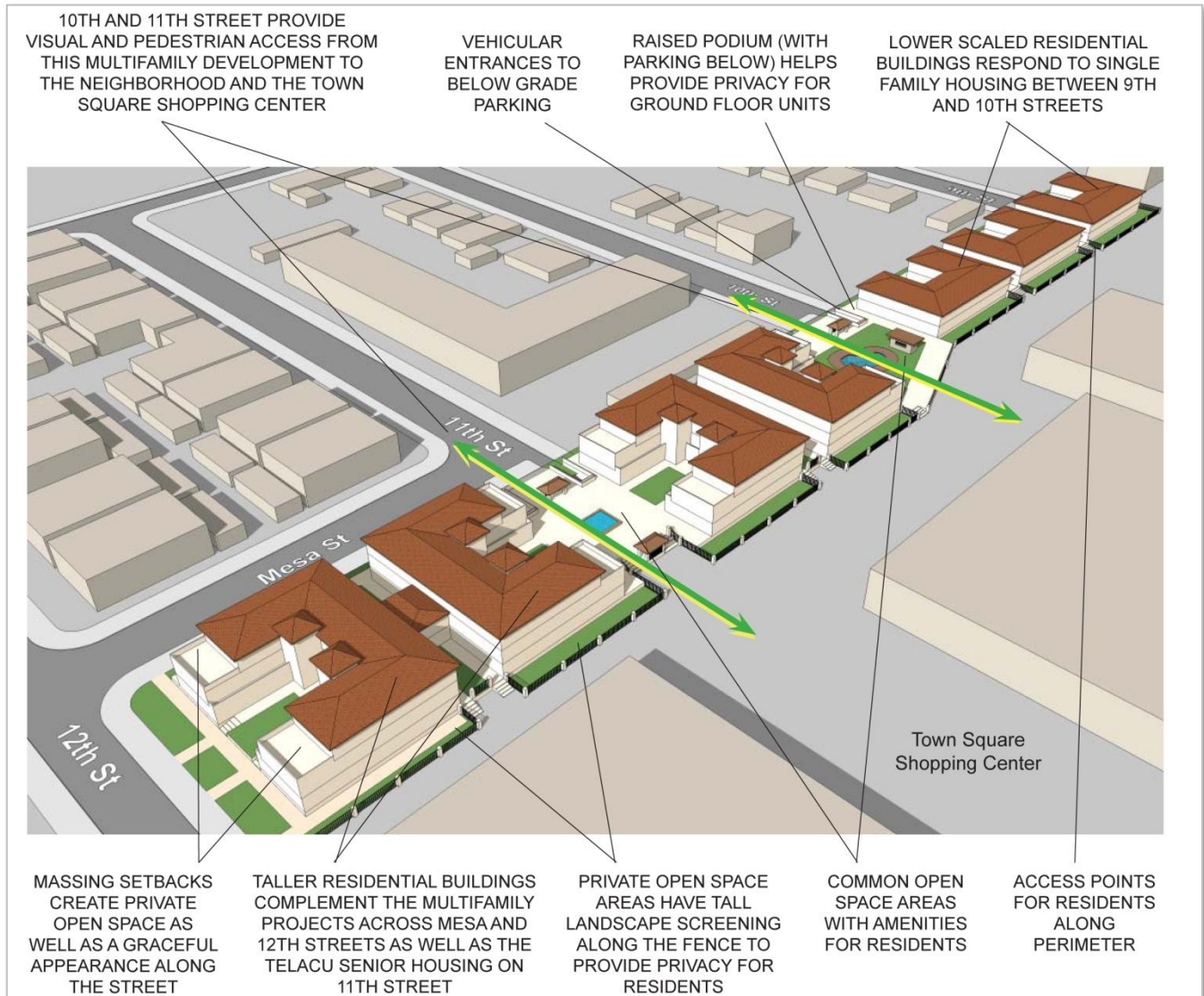


Image 6.6-A: This image illustrates one way an infill multi-family residential project can be developed in the rear of the Town Square Shopping Center on the corner of University Avenue and Chicago Avenue. The illustrated project shown is at 40 units/acre on the approximately 3.5 acre site. Parking is provided below grade and the driveways to the parking are at 10th and 11th Streets, keeping the shopping center and residential traffic separate. Two large common areas (at 10th and 11th Streets) provide amenities for the residents. These two areas also provide visual and pedestrian connections between the residential project, shopping center as well as the residential neighborhood to the east.



Image 6.6-B: This image illustrates a portion of the multi-family residential project as well as an open space area with amenities for the residents. The image also illustrates the vehicular access to the below grade parking. 10th Street, which dead-ends into the project, also serves as a visual and pedestrian connection between the project, neighborhoods, and the Town Center Shopping Center. The use of varied colors and materials, well-articulated building volumes and extensive use of landscaping all contribute to providing an appropriate scale that is responsive to its surroundings.



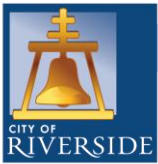
Image 6.6-C: An example of multi-family residential project with "tuck-under" parking.



Image 6.6-D: This example represents an example of the appropriate scale for a residential project in the Commercial Core District.



Image 6.6-E: The pedestrian interface between the residential and commercial uses should be enhanced through an inviting mix of landscaping, hardscaping, and pedestrian-scale amenities such as shade structures, benches and water features.



Permitted Uses

Within the Commercial Core District, the base zone shall dictate the permitted uses for each individual property unless specifically prohibited in the *Prohibited Uses* Section. Base zone permitted uses are found in the Permitted Uses Table, Section 19.150.020(A) of the Zoning Code. All permitted uses are subject to any necessary permits as listed below or in the Zoning Code as well as all other requirements of the Specific Plan and Zoning Code, as applicable. Incidental uses shall be subject to the Incidental Uses Table found in Section 19.150.020(B) of the Zoning Code.

Rezoning for Mixed-Use

As discussed earlier in this chapter, the General Plan Land Use designation of the majority of the properties within the Specific Plan area were changed during the General Plan 2025 Program update to Mixed-Use – Neighborhood (MU-N), Mixed-Use – Village (MU-V), or Mixed-Use – Urban (MU-U) (See Figure 7). Within the Commercial Core District, the land use of the UEI site was designated MU-V, with the Town Square Shopping Center and the Chicago Center Shopping Area both designated MU-U. While the current base zone of each individual property within the Specific Plan area dictates the standards for land use and development, it is strongly recommended that properties be rezoned to the corresponding mixed-use zone per the Zoning Code and in accordance with their respective General Plan Land Use designations, as they are developed. To encourage mixed use development on these properties, it is recommended that the City initiate the necessary zone changes at the time that a viable project is submitted to the Planning Division for design review.

The land use of the two parcels located at the southeasterly corner of this District, comprised of a branch office of the United States Postal Service and a small one-story vacant commercial building, were designated Public Facilities (PF) and Commercial (C), respectively. However, given that these two parcels will now be include as part of the Specific Plan area, it is recommended that the land use designation and zoning be changed to MU-U to encourage mixed-use development on these properties in the future. For properties that adopted the respective mixed-use zone in accordance with their General Plan Land Use Designation, the permitted uses found under the mixed-use zones in the Permitted Uses Table (Section 19.150.020(A)) of the Zoning Code shall apply.

General Plan 2025 Housing Element Conformance

As mentioned in the description of the Commercial Core District, the westerly edge of the Town Square Shopping Center contains a vacant 3.5-acre site that has been identified as a potential infill site under the recent General Plan 2025 Housing Element update. The assumed density under the Housing Element is 40 dwelling units per acre, yielding a total of 98 units. However, the site is capable of being built as a single project at 60 dwelling units per acre. The entire shopping center site has a land use designation of Mixed-Use – Urban, which allows high-density residential uses. However, the current zoning of the shopping center site is Commercial Retail (CR) with an overlay zone of Neighborhood Commercial (NC) for most of the site and a pocket of Commercial General (CG) along the southerly edge, encompassing an indoor shopping center. As such, a city-initiated rezoning to MU-U will be processed for the entire site in conjunction with this update to the University Avenue Specific Plan, consist with the General Plan 2025 Housing Element and in accordance with State of California Planning and Zoning Law.



Prohibited Uses

Within the Commercial Core District, the base zone shall dictate the prohibited uses for each individual property in addition to the uses listed below. In cases where the prohibited uses set forth in the Specific Plan are inconsistent with the Zoning Code, the standards of the Specific Plan shall prevail. Base zone prohibited uses are found in the Permitted Uses Table, Section 19.150.020(A) of the Zoning Code. Any use which is prohibited by state and/or federal law is also strictly prohibited. The uses listed in *Table 6.6A – Commercial Core District Prohibited Uses* below are strictly prohibited within the Commercial Core District.

Table 6.6A – Commercial Core District Prohibited Uses

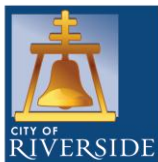
Alcohol Sales – Off-sale ¹
Bail Bonds Offices
Bus Terminal
Group Housing (Convalescent Homes, SRO's, Alcohol & Drug Treatment Facilities, more than 6 Occupants)
Home Improvement, Sales and Service – Retail: 20,000 square feet or More
Homeless Shelters
Pawn Shops
Recycling Facilities
Student Housing
Vehicle Fuel Stations
Vehicle Parts and Accessories
Vehicle Repair Facilities
Vehicle Sales, Rental and Leasing – New and Used
Vehicle Wash Facilities

¹Except as an incidental use to florist shops and establishments that do not propose to sell alcohol as their principal business and contain 15,000 square feet or more of gross floor area. Subject to the Chapter 19.450, Alcohol Sales, of the Zoning Code.

Development Standards

Within the Commercial Core District, the base zone shall dictate the development standards for each individual property with the exception of the standards listed in *Table 6.6B Commercial Core District Development Standards* below. In cases where the development standards set forth in the Specific Plan are inconsistent with the Zoning Code, the standards of the Specific Plan shall prevail.

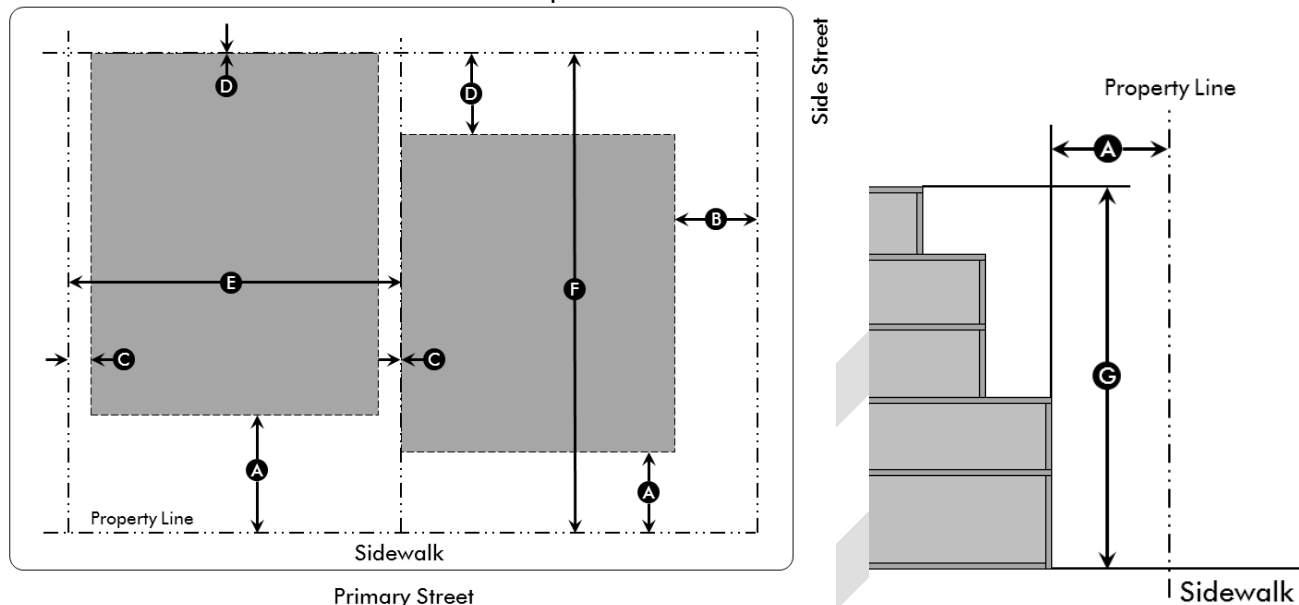




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Table 6.6B Commercial Core District Development Standards



Building Placement			
Setback			
Front – University Avenue ^{1,2}	A	15' – 20'	
Front – Chicago Avenue	A	15' Minimum	
Side Street	B	15' Minimum	
Interior Side ³	C	0' Minimum	
Rear ³	D	0' Minimum	
Minimum Lot Standards			
Lot Area		Base Zone	
Lot Width	E	Base Zone	
Lot Depth	F	Base Zone	
Building Height			
Maximum Height	G	60'	
Standards for Front Setback Area ⁴			
Surface Coverage ⁵			
Hardscaping		50% – 85%	
Landscaping		15% – 50%	
Sidewalk Widening ⁶		3' – 6'	
Additional Standards for Residential as Part of Mixed-Use Development			
Maximum Density			
With less than one acre lot size		20 du/acre	
With one to two acres min. lot size		30 du/acre	
With more than two acres min. lot size		40 du/acre	
Open Space ⁷			
A. Minimum Private Open Space ⁸		50 sq.ft./du for at least 50% of the units	
B. Minimum Common Open Space ⁹		50 sq.ft./du	

Parking	
Mixed-Use	
1.	Parking standards for mixed-use developments shall comply with the Zoning Code, Chapter 19.120.060.B. A reduction in the number of required parking spaces may be permitted for mixed-use developments and/or stand-alone uses in mixed-use zones subject to the approval of a shared parking arrangement.
2.	See Section 6.4 (General Mixed-Use Development and Design Standards) of this Chapter for additional parking standards.

Base Zone	
Parking standards shall comply with the Zoning Code, Chapter 19.580.	

¹See the *Additional Development and Design Standards for the Commercial Core District* Section for additional requirements.

²Applies to building footprint only. Floors can be stepped back to allow for balconies, building articulation and other design features.

³15-foot minimum setback required when abutting a residential use.

⁴Applies to setbacks along University Avenue only.

⁵The entire front setback area, including driveways, shall include a mix of decorative hardscaping and landscaping.

⁶The existing sidewalk shall be widened by an additional 3 to 6 feet with decorative hardscaping along the entire street frontage.

⁷Can be a combination of indoor and outdoor open space for mixed-use developments. Includes patios and balconies.

⁸Private usable space shall have a minimum dimension on any side of 5-feet. Private usable open space can also be met through equivalent design features as approved by the Planning Commission.

⁹Common usable open space may be divided into more than one area; however, each area shall be a minimum of 625-square feet, with no dimension on any side of less than 25-feet.



Additional Development and Design Standards for the Commercial Core District

The following development and design standards apply to all properties within the Commercial Core District.

Front Setback Area

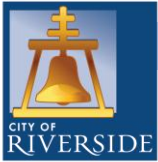
1. Throughout the Commercial Core District, the pedestrian path of travel (e.g. sidewalks) along the street frontage shall be widened by 3 feet to 6 feet (in accordance with the development standards above) through the use of decorative concrete pavers and/or other decorative hard-surface materials to accommodate higher levels of pedestrian foot traffic.
2. In accordance with the developments standards above, a combination of decorative hardscaping and pockets of dense landscaping shall be used along the entire front setback area (including driveways) to create attractive and useable outdoor gathering spaces that encourage indoor activities to relocate to the outside while drawing pedestrian traffic on-site.
3. Hardscaping materials shall include decorative concrete pavers and/or other decorative hard-surface materials that complement the building(s) on site.
4. Landscaping materials shall include dense, yet drought-tolerant, vegetation arranged in pockets, strategically placed to maximize the usable area within the front setback.
5. Landscaping materials shall include large specimens that provide shade and, at the same time, maintain a clear view of building signs from the street frontage.
6. A combination of raised and ground planters as well as decorative pot containers shall be used for all landscaping areas.
7. Tree grates shall be provided for all trees within the sidewalk adjacent to the front setback area to maximize the pedestrian path of travel.
8. Within the front setback area, a minimum of two outdoor amenities listed in Table 6.6C shall be provided:

Table 6.6C – Front Setback Outdoor Amenities

Outdoor Dining	Shade Structures
Water Features	Artistic Bicycle Racks
Art Display Areas	Pave Art
Outdoor Seating	Other Amenities (As deemed acceptable by the Approving Authority)

Building Entrances and Orientation

9. Buildings shall consist of building materials, colors and detailing to fit with the character of the existing surrounding neighborhood.
10. Large-scale projects to reconfigure the shopping centers should be designed with buildings pushed closer to the street to allow for large pedestrian gathering spaces (such as plazas) to be located in the interior of the site.



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11. Primary building entrances should be easy to identify along University Avenue and Chicago Avenue.
12. Buildings should be oriented to a defined pedestrian walkway or street.
13. A series of safe and well designed pedestrian walkways should seamlessly connect the various components within this District to improved pedestrian connectivity.
14. Building materials and colors should exhibit quality and help establish a human scale while providing visual interest.
15. To facilitate the creation of a sense of place, developers are encouraged to incorporate artists into the design team from the inception of planning in order to integrate art into their projects.
16. Special attention should be given to the design of project and building corners as an opportunity to create visual interest and invite activity.
17. Larger sites should feature large plazas and other significant gathering spaces where people can congregate. Public spaces should contribute to the overall sense of place and site identity and help to attract pedestrian users to the development.
18. Loading docks should be located in areas that have the least amount of impact on residential uses.

Building Elevations

19. Ground floor facades of buildings incorporating retail uses should be differentiated from upper stories, allowing for flexibility and change of retail tenants. Retail features, which make up the ground floor façade shall include: retail storefronts; awnings and canopies; sign bands; and cornices. Retail storefronts should be at least 75% glazing and the glazing should be non-tinted and unreflective.
20. New infill should be planned and designed in a manner that protects, sustains, and enhances the existing architectural character of University Avenue.
21. In addition to the setback requirements established, the new infill should match the existing building's width, height and massing.
22. New infill buildings should maintain the size and rhythm of space between buildings along the established streetscape.
23. New mixed-use infill development should conform to the proportion, scale and massing of existing residential or commercial buildings adjacent to the infill development.
24. Larger architectural structures can achieve visual harmony with smaller buildings by the use of repetitive massing, roof forms, materials used and cladding color. New, larger buildings may also be subdivided into smaller elements reflective of the mass or detailing of surrounding buildings (i.e. roof form may employ gables, mansard windows, front porches, colonnades, etc.)



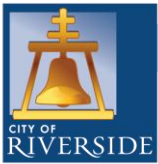
25. The entrances to mixed-use buildings should be always oriented to and be highly visible from the street. Entrances for retail uses must be separated from residential entrances. A well-defined separation between different uses in a mixed-use commercial building is important in order to maintain visual clarity. Additional residential or service entrances connected to the rear parking may be provided from the rear or side of the buildings.
26. Materials and textures applied to the front façade should continue around the building.
27. New buildings and conversions of the existing residential buildings into commercial use should complement the basic façade elements, which include a transparent glazed lower storefront, a sign fascia over a projecting cornice and a solid upper façade punctuated by vertical windows.

Urban Open Spaces – Non Residential Development

28. All new non-residential development on lots of greater than twenty thousand (20,000) square feet must provide urban open space. 1 sq. ft./100 sq. ft. gross floor area or 1 sq. ft./200 sq. ft. lot area, whichever is greater shall be provided as public open space. Such open space shall be required to be:
 - a. Accessible and open to the public.
 - b. Improved with seating, plantings, and/or other amenities.
 - c. Visible and accessible from the street or public pedestrian areas.
 - d. Located on the ground floor or no more than five (5) feet above or five (5) feet below ground level.
 - e. Out of doors, or in the open air (may be under a roof or canopy).
 - f. All required open space shall be located behind the sidewalk and on private property.
 - g. For developments on lots in excess of ten (10) acres (four hundred thirty-five thousand, six hundred (435,600) square feet), at least fifty (50) percent of the required open space must be centrally located on the site in a common area.

Base of High Rise Building

29. For buildings exceeding five (5) stories in height, the following standards shall apply:
 - a. The first floor above street grade shall be distinguished from the remainder of the building with an emphasis on providing design elements that will enhance the pedestrian environment. Such elements as cornices, corbelling, molding, stringcourses, ornamentation, changes in material or color, recessing, architectural lighting and other sculpturing of the base as are appropriate shall be provided to add special interest to the base.
 - b. In the design of the building facade, attention shall be paid to the appearance both during the day and at night. Material and color changes alone do not meet the requirements of this section and design elements, which are used to meet the



requirements of this section, shall be visually continuous around the building. In the event that a building facade is not visible from a public street or right-of-way, then the City Planner, Community Development Director, or their designee (Approving Authority) shall have the option of waiving this requirement.

- c. Special attention shall be given to the design of windows in the base. Band windows are prohibited. Recessed windows that are distinguished from the shaft of the building through the use of arches, pediments, mullions, and other treatments are permitted.

Building Walls Facing a Street

30. All retail and office buildings fronting directly on a public or private street or fronting on a public multi-use path along a transit line and identified in an area plan shall be designed so that the first floor facade of the building(s) along all streets and pathways includes clear glass windows and doors to increase pedestrian interest. These openings shall be arranged so that the uses are visible from and to the street and/or pathway on at least fifty (50) percent of the length of the first floor street level frontage.
31. For all other uses, buildings shall be designed so that the first floor street facade along all streets includes the use of clear glass windows and doors arranged so that the uses are visible from and/or accessible to the street on at least twenty-five (25) percent of the length of the first floor street frontage. When this approach is not feasible, a combination of design elements shall be used on the building facade, or included into the site design, to animate and enliven the streetscape. These design elements may include but are not limited to the following: ornamentation; molding; strong courses; changes in material or color; architectural lighting; works of art; fountains and pools; street furniture; stoops, landscaping and garden areas; and display areas.
32. The first floor facade of all buildings, including structured parking facilities, shall be designed to encourage and complement pedestrian-scale, interest, and activity.
33. Expanses of blank wall shall not exceed twenty (20) continuous feet in length. A blank wall is a facade that does not add to the character of the streetscape and does not contain clear glass windows or doors or sufficient ornamentation, decoration or articulation.
34. No reflective surfaces shall be permitted on street level exterior facades.
35. Ventilation grates on the building, or emergency exit doors located on the first floor street facade(s) shall be decorative and part of the overall building design.

Structured Parking Facilities

36. Structured parking facilities shall be designed to encourage and complement pedestrian-scale interest and activity, and shall be designed so that motorized vehicles parked on all levels of the facility inside are screened from the street, the transitway, and/or from adjacent residentially zoned and/or used property. Decorative elements such as grillwork or louvers may be utilized to accomplish this objective.



37. Openings at the street level are limited to vehicular entrances, pedestrian access to the structure, and ventilation openings. All such openings shall be decorative and be an integral part of the overall building design.

Canopies

38. Canopies, awnings, cornices and similar architectural accents are permitted on exterior building walls. Such features shall be constructed of rigid or flexible material designed to complement the streetscape of the area. Any such feature may extend from the building up to one-half ($\frac{1}{2}$) of the width of the setback area in front of the building or nine (9) feet, whichever is less, and may not be closer than two (2) feet to the back of the curb. In no instance shall such features extend over, or interfere with the growth or maintenance of any required tree plantings. Minimum overhead clearance shall be eight (8) feet. Ground supports for these features are not permitted in the minimum setback, sidewalk or in the public right-of-way. If a canopy, awning, cornice, or other appurtenance extends into the public right-of-way, an encroachment agreement from the City of Riverside Public Works Department shall be required.

Drive-thru Businesses

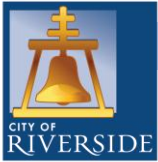
39. Drive-thru businesses shall be allowed in the Commercial Core District with the granting of a Conditional Use Permit (CUP), subject to Chapter 19.475 – Drive-Thru Businesses – of the Zoning Code, in addition to the standards below. In cases where the standards set forth in the Specific Plan are inconsistent with the Zoning Code, the standards of the Specific Plan shall prevail:

General Standards

- a. The proposed site, either currently or proposed, shall be part of a commercial complex of eight acres or greater in size with shared access via a driveway serving the restaurant as well as the greater commercial complex. If the proposed site is on a separate parcel, the parcel should be no less than 30,000 square feet.
- b. A minimum building size of 3,000 square feet of usable interior space.
- c. Associated indoor or outdoor playgrounds shall be prohibited.
- d. Additional driveway openings to a parcel shall be discouraged and existing driveways are to be closed where feasible.
- e. Buildings shall be designed with substantial mass (height and bulk) to create a strong building profile on the property as seen from the street frontage.
- f. A bicycle "Sharrow" lane shall be incorporated into the drive-thru lane of drive-thru restaurants within this District to allow bicyclists to utilize the drive-thru window.

Drive-thru Lane and Window Standards

- g. Drive-thru windows are prohibited on the front building elevation directly facing a street frontage.
- h. No portion of the drive-thru lane shall be located within the front setback area.



- i. Drive thru lanes shall be designed in such a way as to be screened from view from the street through elevation differences, landscaping, arbors, trellises, canopies, walls and other architectural features used to reduce the visual presence of drive-thru operations.

Outdoor Dining Standards

- j. Outdoor dining areas shall be required for all drive-thru restaurants and located fronting on the street frontage to promote pedestrian traffic.
- k. Outdoor dining areas shall be subject to the *Front Setback Area* standards listed above.

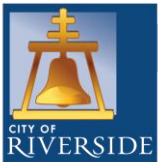
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Figure 20
Chicago-Iowa District

Map of the Chicago-Iowa District. The district is highlighted in blue. Streets shown include Ottawa Av, Loma Vista St, Louella Ln, Seventh St, University Av, Ninth St, Tenth St, Chicago Av, Presley Av, Linden St, Niki Wy, Cranford Av, and Iowa Av. Buildings are shown in grey, and parcels in white. A legend at the bottom left identifies these symbols. A north arrow and a scale bar (0 to 500 feet) are at the bottom right.

Encompassing approximately 79 acres within the University Avenue Specific Plan area, the Chicago-Iowa District is primarily located in the University Neighborhood and is by far the largest of the four Districts and the most diverse in terms of land uses. The western edge of this District, partially located in the Eastside Neighborhood, extends beyond Chicago Avenue to Mesa Street along the northerly side of University Avenue and contains several neighborhood-serving uses located on mid-size lots with single-story buildings, including restaurants, retail uses, a bank and the newly constructed Walgreen's Pharmacy site.

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The eastern portions of this District, situated between Cranford Avenue and Iowa Avenue, contain a mix of uses, including restaurants, office and retail uses, personal services, a vehicle fueling station and several high-density residential developments comprised of multi-family, senior and student housing. Also located within this District is the vacant Farm House Motel. Constructed in 1953, the motel is an excellent example of a motor court and eligible for designation as a local landmark and for listing in the National Register of Historic Places, based on a recent cultural resources survey of properties along the University Avenue corridor. The student housing developments include the 760-bedroom Grand Marc at University Village, located on Iowa Avenue, and the Palms on University, a mixed-use site with 525 student bedrooms and 16,690 square feet of ground-floor retail space located on University Avenue. The previous University Avenue Specific Plan area excluded several of the multi-family residential sites within this District due to the fact that the sites did not front on University Avenue. However, in recent years it has become apparent that the sites have a strong connection to activities along the University Avenue corridor as well as the surrounding community. Further, with the recent update to the UCR Long Range Development Plan (LRDP) and the anticipated School of Medicine, proposed to be located in close proximity, it is logical to include the subject sites, particularly those located on the southerly side of University Avenue, as part of the Specific Plan area.

With its diverse set of uses and gradual increase in use intensity and development patterns between the western and eastern portions, the Chicago-Iowa District serves as a transition area between the higher intensity uses located in the University Village District to the east and the smaller-scale uses in the Eastside Neighborhood District to the west. In more recent years, this District has undergone a transformation from uses associated with the automobile and lodging industry to more neighborhood-serving uses and high-density residential developments, particularly student housing. The Grand Marc at University Village student housing project on Iowa Avenue was one of the initial catalyst projects to begin this transformation. More recently, the Palms on University student housing project has set a precedent as the only true vertical mixed-use project containing a residential component within the Specific Plan area. Further, the new high-quality Walgreen's Pharmacy project, centrally located near the intersection of University Avenue and Chicago Avenue, has brought another neighborhood-serving use to this District that is easily accessible to the surrounding Eastside and University Neighborhoods as well as the UCR community. Finally, a major streetscape and median enhancement program was recently completed along the entire University Avenue corridor.

With the anticipated growth of the UCR campus, particularly the west campus between Chicago Avenue and the I-215 Freeway (see Figure 6), it is critical that the Chicago-Iowa District continue to serve as the transition area between the higher intensity uses located in the University Village District to the east, as well as the anticipated growth of the UCR west campus to the south and southeast, and the smaller-scale uses in the Eastside Neighborhood District to the west. The larger lots located in the central and eastern portions of this District should be developed with a mix of uses, with an emphasis on residential and entertainment uses to serve the UCR community and surrounding neighborhoods. Properties that are eligible for historic preservation should be adaptively reused with similar uses. New development in this area should include larger scale, mixed-use buildings with multiple floors that sit closer to the street and feature retail and entertainment uses on the ground floor and residential or office uses on the floors above. Given the existing high density residential uses in this District and the proximity to University Village and the UCR campus, the interface between the avenue and the properties along the street frontage should be designed to accommodate higher levels of bicycle and pedestrian foot traffic. The front setback areas in this District should feature a greater hard-surface expansion of the sidewalk than the Eastside Neighborhood and



Commercial Core Districts. The expansion should be completed in combination with decorative hardscaping and dense landscaping within the front setback area. Larger size buildings with multiple stories should be placed near the property line with the ground floor setback further to allow for arcades that can accommodate attractive and useable outdoor gathering spaces featuring decorative hardscaping in combination with landscaping in planters and container pots to encourage indoor activities to relocate to the outside while drawing pedestrian traffic on-site.

Chicago-Iowa District Illustrated

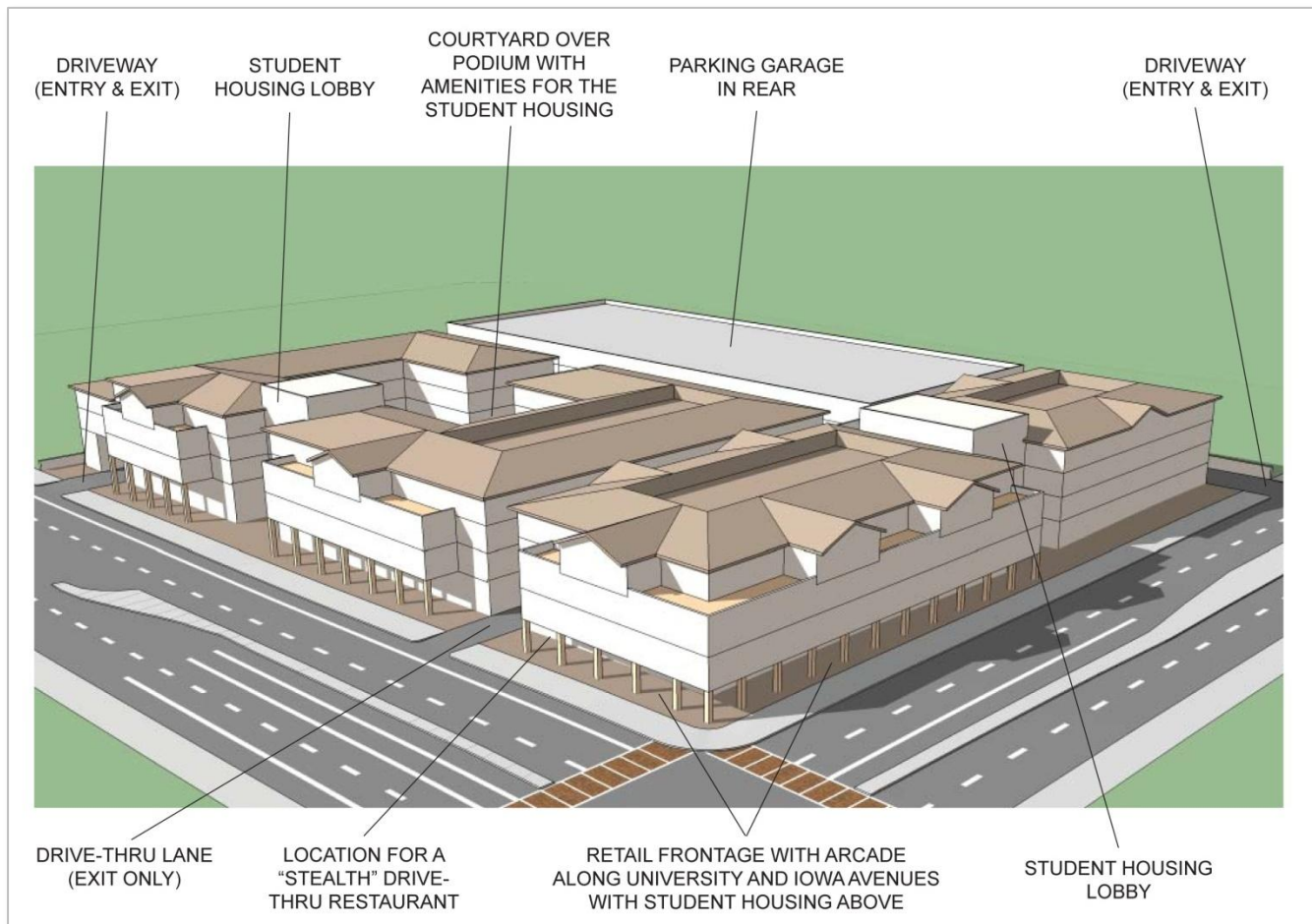


Image 6.7-A: This image illustrates one way this prime site on the corner of University Avenue and Iowa Avenue may be developed as a mixed use project that accommodates a retail frontage along the two major streets with a “stealth” drive-thru restaurant on the corner, student housing above and structured parking in the rear of the site to provide the requisite parking. The “stealth” drive-thru restaurant is shown on the corner of University and Iowa with outdoor dining under the arcade and an adjacent drive-thru exit lane. Separate driveways at opposing ends of the project accommodate the entries and non-restaurant exits for the project. The massing model also illustrates the development standards for this district and how an arcade can create shaded walkways for pedestrians as well as spaces for outdoor dining.



Image 6.7-B: This image illustrates how a mixed use project on this prime site in the Chicago-Iowa District can successfully engage the public realm at the street level while still accommodating uses such as a drive-thru restaurant and residential uses at the upper levels. Building articulation and modulation and the use of varied materials are key factors in providing a human scale to a fairly large project. Street trees, landscaping along the arcade and outdoor dining, all provide shade and urban amenities along the street.



Image 6.7-C: This is an example of the appropriate size and scale for the Chicago-Iowa District.



Image 6.7-D: Arcades can accommodate attractive and useable outdoor gathering spaces featuring decorative hardscaping in combination with landscaping in planters and container pots to encourage outdoor gathering and activate the street.



Permitted Uses

Within the Chicago-Iowa District, the base zone shall dictate the permitted uses for each individual property unless specifically prohibited in the *Prohibited Uses* Section below. In cases where the permitted uses set forth in the Specific Plan for this District are inconsistent with the Zoning Code, the standards of the Specific Plan shall prevail. Base zone permitted uses are found in the Permitted Uses Table, Section 19.150.020(A) of the Zoning Code. All permitted uses are subject to any necessary permits as listed below or in the Zoning Code as well as all other requirements of the Specific Plan and Zoning Code, as applicable. Incidental uses shall be subject to the Incidental Uses Table found in Section 19.150.020(B) of the Zoning Code.

Rezoning for Mixed-Use

As discussed earlier in this chapter, the General Plan Land Use designation of the majority of the properties within the Specific Plan area were change during the General Plan 2025 Program update to Mixed-Use – Neighborhood (MU-N), Mixed-Use – Village (MU-V), or Mixed-Use – Urban (MU-U) (See Figure 7). Within the Chicago-Iowa District, the land use of the properties located west of Chicago Avenue were designated MU-V, with the majority of the properties located east of Chicago Avenue and along University Avenue and Iowa Avenue designated MU-U. While the current base zone of each individual property within the Specific Plan area dictates the standards for land use and development, it is strongly recommended that properties be rezoned to the corresponding mixed-use zone per the Zoning Code and in accordance with their respective General Plan Land Use designations, as they are developed. To encourage mixed use development on these properties, it is recommended that the City initiate the necessary zone changes at the time that a viable project is submitted to the Planning Division for design review.

The land use of several properties located along the southerly boundary of this District, comprised of multi-family residential uses, were designated High Density Residential (HDR). Similarly, the land use of northern half of a 4.3-acre underutilized parcel containing a 24,000 square-foot building located on University Avenue was designated Business/Office Park (B/OP). However, to keep consistency with the land use designation of the surrounding properties in this District, it is recommended that the land use designation and zoning be changed to MU-U to encourage mixed-use development on these properties in the future. For properties that adopted the respective mixed-use zone in accordance with their General Plan Land Use Designation, the permitted uses found under the mixed-use zones in the Permitted Uses Table (Section 19.150.020(A)) of the Zoning Code shall apply.

Prohibited Uses

Within the Chicago-Iowa District, the base zone shall dictate the prohibited uses for each individual property in addition to the uses listed below. In cases where the prohibited uses set forth in the Specific Plan are inconsistent with the Zoning Code, the standards of the Specific Plan shall prevail. Base zone prohibited uses are found in the Permitted Uses Table, Section 19.150.020(A) of the Zoning Code. Any use which is prohibited by state and/or federal law is also strictly prohibited. The uses listed in *Table 6.7A – Chicago-Iowa District Prohibited Uses* below are strictly prohibited within the Chicago-Iowa District.



Table 6.7A – Chicago-Iowa District Prohibited Uses

Alcohol Sales – Off-sale ¹
Ambulance Company with Vehicle Storage
Bail Bonds Offices
Boardinghouse
Bus Terminal
Commercial Laundry
Group Housing (Convalescent Homes, SRO's, Alcohol & Drug Treatment Facilities, more than 6 Occupants)
Home Improvement, Sales and Service – Retail: 20,000 square feet or More
Homeless Shelters
Pawn Shops
Recycling Facilities
Vehicle Fuel Stations
Vehicle Parts and Accessories
Vehicle Repair Facilities
Vehicle Sales, Rental and Leasing – New and Used
Vehicle Wash Facilities

¹Except as an incidental use to florist shops and establishments that do not propose to sell alcohol as their principal business and contain 15,000 square feet or more of gross floor area. Subject to the Chapter 19.450, Alcohol Sales, of the Zoning Code.

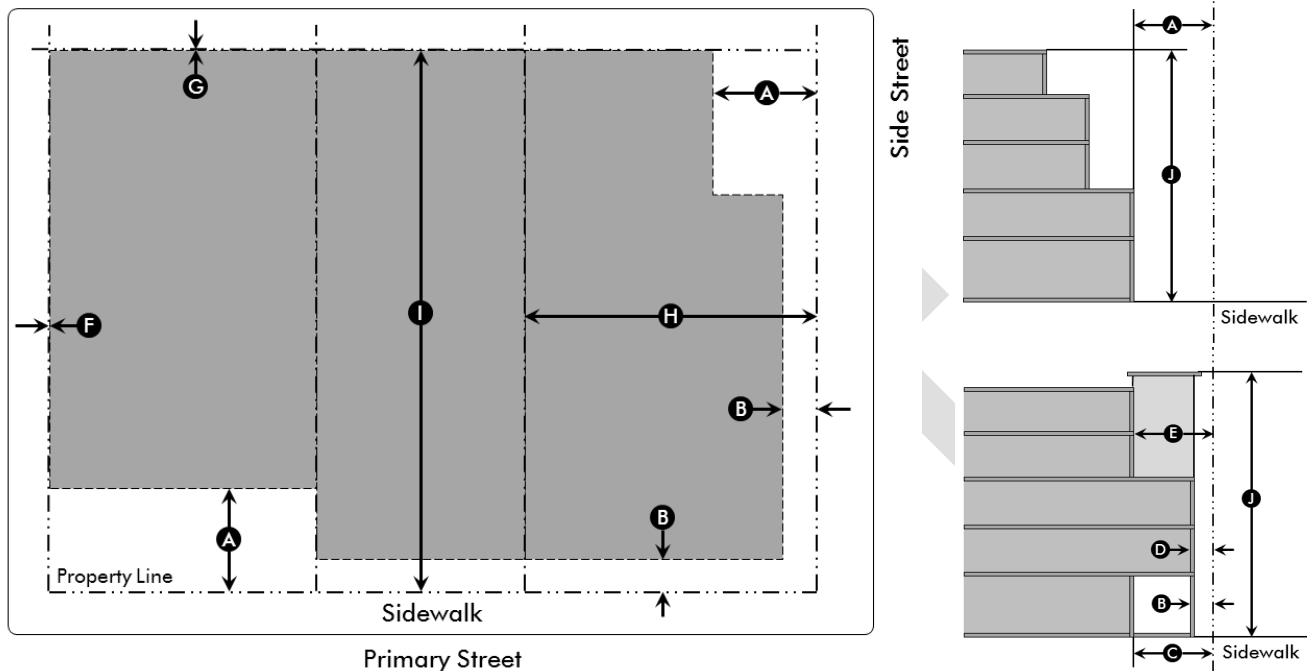
Development Standards

Within the Chicago-Iowa District, the base zone shall dictate the development standards for each individual property with the exception of the standards listed in *Table 6.7B Chicago-Iowa District Development Standards* below. In cases where the development standards set forth in the Specific Plan are inconsistent with the Zoning Code, the standards of the Specific Plan shall prevail.





Table 6.7B Chicago-Iowa District Development Standards



Building Placement		
Setback		
Front/Side Street ^{1,2}	A	15' – 20'
Front/Side Street with Arcade ^{1,2}	B	5' – 10'
Ground Floor	C	15' Minimum
2 nd and 3 rd Floors	D	5' Minimum
4 th Floor and Above ³	E	15' Minimum
Interior Side ⁴	F	0' Minimum
Rear ⁴	G	0' Minimum
Minimum Lot Standards		
Lot Area		Base Zone
Lot Width	H	Base Zone
Lot Depth	I	Base Zone
Maximum Building Height		
West of Chicago Avenue		45'
East of Chicago Avenue	J	60'
Front/Side Street Setback Area Standards ^{1,5}		
Surface Coverage ⁶		
Hardscaping		50% – 85%
Landscaping		15% – 50%
Sidewalk Widening ⁷		3' – 9'
Additional Standards for Residential as Part of Mixed-Use Development		
Maximum Density		
With less than one acre lot size		20 du/acre
With one to two acres min. lot size		40 du/acre
With more than two acres min. lot size		60 du/acre
Open Space ⁸		
A. Minimum Private Open Space ⁹		50 sq.ft./du for at least 50% of the units

Additional Standards for Residential as Part of Mixed-Use Development

B. Minimum Common Open Space¹⁰ 50 sq.ft./du

Parking Mixed-Use

- Parking standards for mixed-use developments shall comply with the Zoning Code, Chapter 19.120.060.B. A reduction in the number of required parking spaces may be permitted for mixed-use developments and/or stand-alone uses in mixed-use zones subject to the approval of a shared parking arrangement.
- See Section 6.4 (General Mixed-Use Development and Design Standards) of this Chapter for additional parking standards.

Base Zone

Parking standards shall comply with the Zoning Code, Chapter 19.580.

Notes

¹See the *Additional Development and Design Standards for the Chicago-Iowa District* Section for additional requirements.

²Applies to building footprint only. Floors can be stepped back to allow for balconies, building articulation and other design features.

³Maximum of 20% of frontage is allowed a minimum 5-foot setback.

⁴15-foot minimum setback required when abutting a residential use.

⁵Applies to setbacks along University Avenue, Chicago Avenue and Iowa Avenue only.

⁶The entire front/side street setback area, including driveways, shall include a mix of decorative hardscaping and landscaping.



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⁷The existing sidewalk shall be widened by an additional 3 to 9 feet with decorative hardscaping along the entire street frontage.

⁸Can be a combination of indoor and outdoor open space for mixed-use developments. Includes patios and balconies.

⁹Private usable space shall have a minimum dimension on any side of 5-feet. Private usable open space can also be met through equivalent design features as approved by the Planning Commission.

¹⁰Common usable open space may be divided into more than one area; however, each area shall be a minimum of 625-square feet, with no dimension on any side of less than 25-feet.

Additional Development and Design Standards for the Chicago-Iowa District

The following development and design standards apply to all properties within the Chicago-Iowa District.

Front Setback Area (See Figure 21)

1. Throughout the Chicago-Iowa District, the pedestrian path of travel (e.g. sidewalks) along the street frontage shall be widened by 3 feet to 9 feet (in accordance with the development standards above) through the use of decorative concrete pavers and/or other decorative hard-surface materials to accommodate higher levels of pedestrian foot traffic.
2. In accordance with the developments standards above, a combination of decorative hardscaping and pockets of dense landscaping shall be used along the entire front setback area (including driveways) to create attractive and useable outdoor gathering spaces that encourage indoor activities to relocate to the outside while drawing pedestrian traffic on-site.
3. Building setbacks for projects that incorporate an arcade along the street frontage can be reduced to a minimum of 5 feet and a maximum of 10 feet to accommodate attractive and useable outdoor gathering spaces, in accordance with the standards in Table 6.8B above.
4. Hardscaping materials shall include decorative concrete pavers and/or other decorative hard-surface materials that complement the building(s) on site.
5. Landscaping materials shall include dense, yet drought-tolerant, vegetation arranged in pockets, strategically placed to maximize the usable area within the front setback.
6. Landscaping materials shall include large specimens that provide shade and, at the same time, maintain a clear view of building signs from the street frontage.
7. A combination of raised and ground planters as well as decorative pot containers shall be used for all landscaping areas.
8. Tree grates shall be provided for all trees within the sidewalk adjacent to the front setback area to maximize the pedestrian path of travel.
9. Within the front setback area, a minimum of two outdoor amenities listed in Table 6.7C shall be provided:

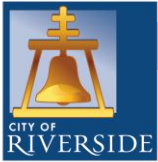
Table 6.7C – Front Setback Outdoor Amenities

Outdoor Dining	Shade Structures
Water Features	Artistic Bicycle Racks
Art Display Areas	Pave Art
Outdoor Seating	Other Amenities (As deemed acceptable by the Approving Authority)

Figure 21

Front Setback Area Treatment for Project with Arcades





Building Elevations

10. Ground floor facades of buildings incorporating retail uses should be differentiated from upper stories, allowing for flexibility and change of retail tenants. Retail features, which make up the ground floor façade shall include: retail storefronts; awnings and canopies; sign bands; and cornices. Retail storefronts should be at least 75% glazing and the glazing should be non-tinted and unreflective.
11. Larger buildings with multiple stories should incorporate step-backs into the design of the building elevation(s) facing the street, in accordance with the step-back standards in Table 6.8B above.
12. New infill development should be planned and designed in a manner that protects, sustains, and enhances the existing architectural character of University Avenue.
13. In addition to the established setback requirements, new additions to existing buildings should complement the existing building's width, height and massing.
14. New infill buildings should maintain the size and rhythm of space between buildings along the established streetscape.
15. New mixed-use infill development should conform to the proportion, scale and massing of existing residential or commercial buildings adjacent to the infill development.
16. Larger architectural structures can achieve visual harmony with smaller buildings by the use of repetitive massing, roof forms, materials used and cladding color. New, larger buildings may also be subdivided into smaller elements reflective of the mass or detailing of surrounding buildings (i.e. roof form may employ gables, mansard windows, front porches, colonnades, etc.)
17. The entrances to mixed-use buildings should be always oriented to and be highly visible from the street. Entrances for retail uses must be separated from residential entrances. A well-defined separation between different uses in a mixed-use commercial building is important in order to maintain visual clarity. Additional residential or service entrances connected to the rear parking may be provided from the rear or side of the buildings.
18. Materials and textures applied to the front façade should continue around the building.
19. New buildings should complement the basic façade elements, which include a transparent glazed lower storefront, a sign fascia over a projecting cornice and a solid upper façade punctuated by vertical windows.

Building Entrances and Orientation

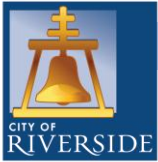
20. Buildings shall consist of building materials, colors and detailing to fit with the character of the existing surrounding neighborhood.
21. Large-scale projects to reconfigure the shopping centers should be designed with buildings pushed closer to the street to allow for large pedestrian gathering spaces (such as plazas) to be located in the interior of the site.



22. Primary building entrances should be easy to identify along University Avenue and Chicago Avenue.
23. Buildings should be oriented to a defined pedestrian walkway or street.
24. A series of safe and well designed pedestrian walkways should seamlessly connect the various components within this District to improved pedestrian connectivity.
25. Building materials and colors should exhibit quality and help establish a human scale while providing visual interest.
26. To facilitate the creation of a sense of place, developers are encouraged to incorporate artists into the design team from the inception of planning in order to integrate art into their projects.
27. Special attention should be given to the design of project and building corners as an opportunity to create visual interest and invite activity.
28. Larger sites should feature large plazas and other significant gathering spaces where people can congregate. Public spaces should contribute to the overall sense of place and site identity and help to attract pedestrian users to the development.
29. Loading docks should be located in areas that have the least amount of impact on residential uses.

Urban Open Spaces – Non Residential Development

30. All new non-residential development on lots of greater than twenty thousand (20,000) square feet must provide urban open space. 1 sq. ft./100 sq. ft. gross floor area or 1 sq. ft./200 sq. ft. lot area, whichever is greater shall be provided as public open space. Such open space shall be required to be:
 - a. Accessible and open to the public.
 - b. Improved with seating, plantings, and/or other amenities.
 - c. Visible and accessible from the street or public pedestrian areas.
 - d. Located on the ground floor or no more than five (5) feet above or five (5) feet below ground level.
 - e. Out of doors, or in the open air (may be under a roof, canopy or arcade).
 - f. All required open space shall be located on private property.
 - g. For developments on lots in excess of ten (10) acres (four hundred thirty-five thousand, six hundred (435,600) square feet), at least fifty (50) percent of the required open space must be centrally located on the site in a common area.



Base of High Rise Building

31. For buildings exceeding five (5) stories in height, the following standards shall apply:
- The first floor above street grade shall be distinguished from the remainder of the building with an emphasis on providing design elements that will enhance the pedestrian environment. Such elements as cornices, corbelling, molding, stringcourses, ornamentation, changes in material or color, recessing, architectural lighting and other sculpturing of the base as are appropriate shall be provided to add special interest to the base.
 - In the design of the building facade, attention shall be paid to the appearance both during the day and at night. Material and color changes alone do not meet the requirements of this section and design elements, which are used to meet the requirements of this section, shall be visually continuous around the building. In the event that a building facade is not visible from a public street or right-of-way, then the City Planner, Community Development Director, or their designee (Approving Authority) shall have the option of waiving this requirement.
 - Special attention shall be given to the design of windows in the base. Band windows are prohibited. Recessed windows that are distinguished from the shaft of the building through the use of arches, pediments, mullions, and other treatments are permitted.

Building Walls Facing a Street

32. All retail and office buildings fronting directly on a public or private street or fronting on a public multi-use path along a transit line and identified in an area plan shall be designed so that the first floor facade of the building(s) along all streets and pathways includes clear glass windows and doors to increase pedestrian interest. These openings shall be arranged so that the uses are visible from and to the street and/or pathway on at least fifty (50) percent of the length of the first floor street level frontage.
33. For all other uses, buildings shall be designed so that the first floor street facade along all streets includes the use of clear glass windows and doors arranged so that the uses are visible from and/or accessible to the street on at least twenty-five (25) percent of the length of the first floor street frontage. When this approach is not feasible, a combination of design elements shall be used on the building facade, or included into the site design, to animate and enliven the streetscape. These design elements may include but are not limited to the following: ornamentation; molding; strong courses; changes in material or color; architectural lighting; works of art; fountains and pools; street furniture; stoops, landscaping and garden areas; and display areas.
34. The first floor facade of all buildings, including structured parking facilities, shall be designed to encourage and complement pedestrian-scale, interest, and activity.
35. Expanses of blank wall shall not exceed twenty (20) continuous feet in length. A blank wall is a facade that does not add to the character of the streetscape and does not contain clear glass windows or doors or sufficient ornamentation, decoration or articulation.



36. No reflective surfaces shall be permitted on street level exterior facades.
37. Ventilation grates on the building, or emergency exit doors located on the first floor street facade(s) shall be decorative and part of the overall building design.

Structured Parking Facilities

38. Structured parking facilities shall be designed to encourage and complement pedestrian-scale interest and activity, and shall be designed so that motorized vehicles parked on all levels of the facility inside are screened from the street, the transitway, and/or from adjacent residentially zoned and/or used property. Decorative elements such as grillwork or louvers may be utilized to accomplish this objective.
39. Openings at the street level are limited to vehicular entrances, pedestrian access to the structure, and ventilation openings. All such openings shall be decorative and be an integral part of the overall building design.

Canopies

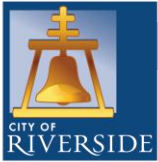
40. Canopies, awnings, cornices and similar architectural accents are permitted on exterior building walls. Such features shall be constructed of rigid or flexible material designed to complement the streetscape of the area. Any such feature may extend from the building up to one-half ($\frac{1}{2}$) of the width of the setback area in front of the building or nine (9) feet, whichever is less, and may not be closer than two (2) feet to the back of the curb. In no instance shall such features extend over, or interfere with the growth or maintenance of any required tree plantings. Minimum overhead clearance shall be eight (8) feet. Ground supports for these features are not permitted in the minimum setback, sidewalk or in the public right-of-way. If a canopy, awning, cornice, or other appurtenance extends into the public right-of-way, an encroachment agreement from the City of Riverside Public Works Department shall be required.

Drive-thru Businesses

41. Drive-thru businesses shall be allowed in the Chicago-Iowa District with the granting of a Conditional Use Permit (CUP), subject to Chapter 19.475 – Drive-Thru Businesses – of the Zoning Code, in addition to the standards below. In cases where the standards set forth in the Specific Plan are inconsistent with the Zoning Code, the standards of the Specific Plan shall prevail:

General Standards

- a. The proposed site shall be located on a parcel of at least 30,000 square feet in area.
- b. The proposed site shall be located on a parcel that maintains a minimum of 100 feet on all street frontage.
- c. The proposed site shall be located on a parcel that is adjacent to the intersection of two (2) arterials streets, as indicated by the Riverside General Plan 2025, Circulation Element – Master Plan of Roadways.
- d. Associated indoor or outdoor playgrounds shall be prohibited.



- e. Additional driveway openings to a parcel shall be discouraged and existing driveways are to be closed where feasible.
- f. Buildings shall be designed with substantial mass (height and bulk) to create a strong building profile on the property as seen from the street frontage.

Drive-thru Lane and Window Standards

- g. Drive-thru windows are prohibited on the front building elevation directly facing a street frontage.
- h. No portion of the drive-thru lane shall be located within the front setback area.
- i. Drive thru lanes shall be designed in such a way as to be screened from view from the street through elevation differences, landscaping, arbors, trellises, canopies, walls and other architectural features used to reduce the visual presence of drive-thru operations.
- j. A bicycle "Sharrows" lane shall be incorporated into the drive-thru lane of drive-thru restaurants within this District to allow bicyclists to utilize the drive-thru window.

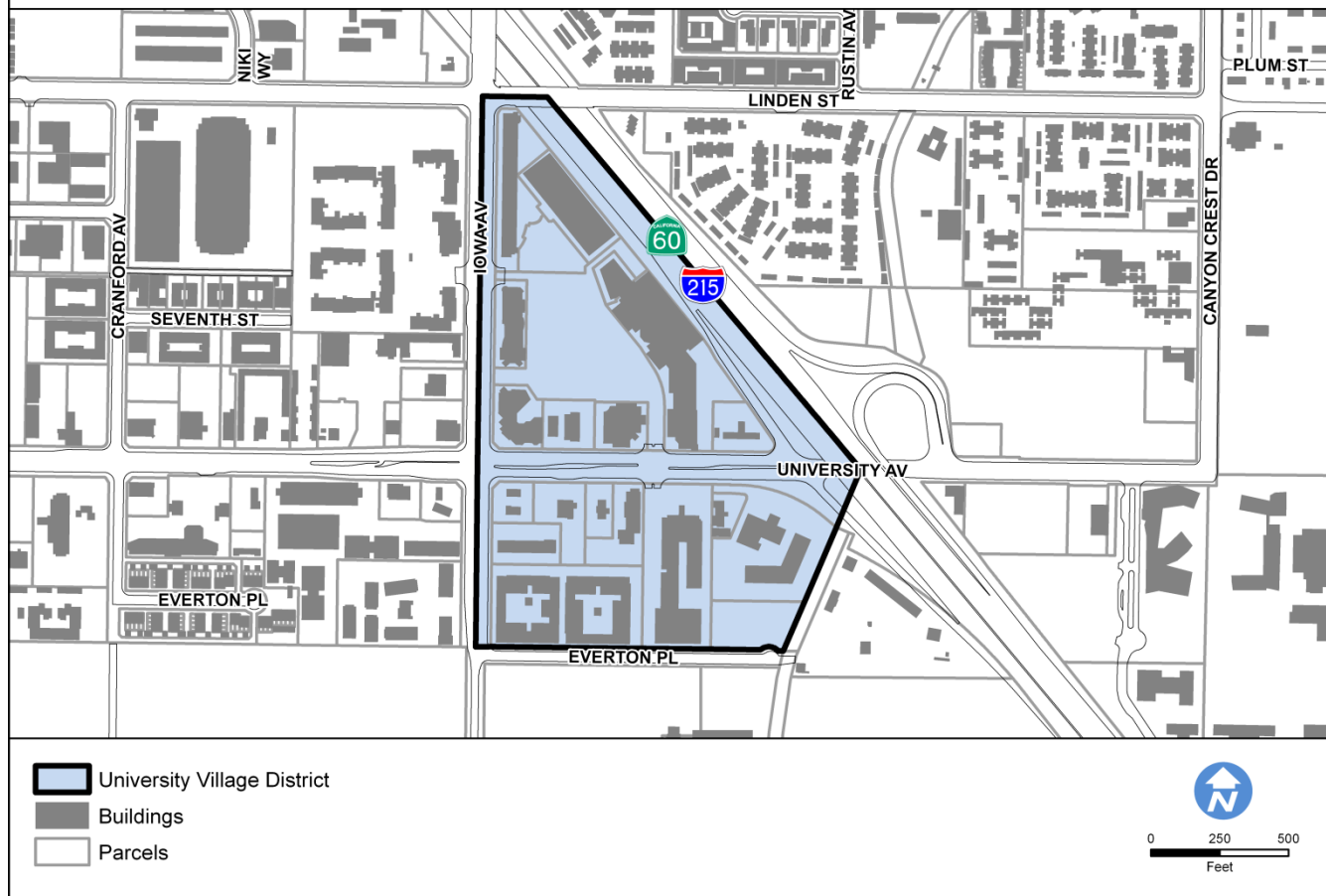
Outdoor Dining Standards

- k. Outdoor dining areas shall be required for all drive-thru restaurants and located fronting on the street frontage to promote pedestrian traffic.
- l. Outdoor dining areas shall be subject to the *Front Setback Area* standards listed above.



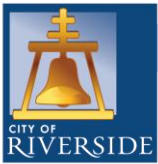
6.8 University Village District

Figure 21
University Village District



District Description

The University Village District, and in particular the University Village site, has come a long way from when the University Avenue Specific Plan was first adopted in 1993. The University Village site, bounded by, Linden Street to the north, Iowa Avenue to the west, the I-215 Freeway to the east and University Avenue to the south, is comprised of ten individual parcels that function as a one cohesive and integrated development. The largest and most prominent features within this site include the University Village Towers, an eight-story 525-bedroom student housing development, a six-story parking structure, both located in the northern portion of the site, and a large two-story multiplex movie theatre located in the southeast portion of the site. The site also contains a diverse mix of additional student-serving uses including restaurants, personal services, office, retail and entertainment uses, primarily located in several one to two-story buildings along the street frontages of University Avenue and Iowa Avenue. The interior of the site is comprised of a surface parking lot surrounded by various outdoor seating areas and open-air plazas. A vacant 0.82-acre infill lot is also located within the interior of the site, adjacent to the University Village Towers and the parking structure. The only other site located north of University Avenue and within this district is a 0.74-acre parcel containing a vehicle fueling station, fronting on University Avenue between University Village and the I-215 Freeway.



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The southern portion of the University Village District, bounded by University Avenue to the north, Iowa Avenue to the west, the I-215 Freeway to the east and the UCR campus to the south, contains several buildings owned and operated by the University of California, including the UCR Extension Center. The buildings are located on larger size parcels and range from one to five stories, with the largest being the UCR Extension Center building. The southern portion of the University Village District also contains a vehicle fueling station and retail uses comprised mostly of fast-food restaurants located near the intersection of University Avenue and Iowa Avenue, and a motel and multi-family residential development, both located along Iowa Avenue.

The previous University Avenue Specific Plan area excluded the multi-family residential development site along Iowa Avenue due to the fact that the site did not front on University Avenue. However, in recent years it has become apparent that the site has a strong connection to the activities along the University Avenue corridor as well as the surrounding community. Further, with the recent update to the UCR Long Range Development Plan and the anticipated School of Medicine, proposed to be located in close proximity, it is logical to include the subject site, as part of the Specific Plan area. Finally, while not located within the Specific Plan area, the Gage Canal runs immediately adjacent to the west of this District, through the UCR Campus.

The development of University Village over the past 20 years has been a concerted effort between the City, the development community, the surrounding neighborhoods and the UCR community. With its mix of uses, gathering spaces, identifiable entrances and buildings situated along the street frontages, University Village has become a major hub for the UCR Community as well as the surrounding neighborhoods. While the northern portion of the University Village District has seen the most transformation, changes in the southern portion have primarily occurred through the adaptive reuse of existing buildings. For example, the UCR Extension Center and a research center, both owned and operated by the University of California, currently occupy buildings that were originally occupied by a hotel and motel, respectively. More recently, a major streetscape and median enhancement program was also completed along the entire University Avenue corridor.

With the anticipated growth of the UCR campus and the future School of Medicine, proposed to be located on the west campus immediately to the south, the University Village District has the potential to become the next town-and-gown village in the tradition of Westwood Village at UCLA or Telegraph Avenue in Berkeley. Critical to this vision are the planning efforts between the City and the University of California in the coming years, particularly the coordinated efforts between of the University Avenue Specific Plan and the UCR Long Range Development Plan (LRDP).

The UCR LRDP proposes that the vacant land located immediately south of the Specific Plan area should develop with academic buildings, multi-family housing, recreational uses, parking and other campus support uses. It also proposes to place the Gage Canal in an underground pipe and create a linear open space area that meanders through the west campus and connects Martin Luther King Boulevard to the south with University Avenue to the north. While the boundaries of this District and the UCR Campus abut, the connectivity between the two areas is severely hindered by the I-215 Freeway, particularly for pedestrians and bicyclists. Efforts to increase connectivity should focus on improvements to the University Avenue underpass at the I-215 Freeway as well as the connections to the west campus through Iowa Avenue and the Gage Canal. Identifiable gateways and way-finding markers should be provided to encourage pedestrians and bicyclist use



these routes between the two areas. Future projects near the common boundary of both planning areas should be allowed to development seamlessly between both areas.

Similarly, the northern and southern portion of the University Village District should development with uses and development patterns that seamlessly connect to each other as well as to the UCR campus. Uses and development patterns in the northern portion of this District should continue to allow for more student-oriented dining, shopping and entertainment opportunities in conjunction with high-density mixed-use developments that feature prominent residential components. To accommodate these uses, the surface parking lot in the interior of University Village should be developed with additional buildings in combination with open-air plazas, outdoor gathering spaces and drive aisles with angled parking that serve bicyclists, motorists and delivery vehicles. The vacant 0.82 infill lot near the University Village Towers and the parking structure should be converted into a pocket park that serves as an event plaza.

The images below illustrate the concept plan for the University Village site. Within the southern portion of this District, the five smaller lots located near the intersection of University Avenue and Iowa Avenue should be consolidated to allow a cohesive mixed-use project to development. The interface between University and Iowa Avenues and the properties along the street frontage should be designed to accommodate higher levels of bicycle and pedestrian foot traffic. The front setback areas in this District should feature a greater hard-surface expansion of the sidewalk than the Eastside Neighborhood and Commercial Core Districts. The expansion should be completed in combination with decorative hardscaping and dense landscaping within the front setback area. Larger size buildings with multiple stories should be placed near the property line with the ground floor setback further to allow for arcades that can accommodate attractive and useable outdoor gathering spaces featuring decorative hardscaping in combination with landscaping in planters and container pots to encourage indoor activities to relocate to the outside while drawing pedestrian traffic on-site.

University Village District Illustrated



Image 6.8-A: This image depicts an example of the appropriate scale and massing for the south side of University Avenue in the University Village District.



Image 6.8-B: The surface parking lot in the interior of University Village should be developed with additional buildings in combination with open-air plazas, outdoor gathering spaces and drive aisles with angled parking.



Image 6.8-C: The existing vacant triangular parcel is an ideal location to accommodate a pocket park/events plaza.



Image 6.8-D: Reconfiguring the existing interior surface parking lot with commercial/retail buildings and pedestrian amenities will further enhance the appeal of University Village.



Image 6.8-E



Images 6.8-E, 6.8-F, and 6.8-G illustrate how the streets can be activated through a creative combination of landscape materials, hardscaping, public art, pedestrian amenities, outdoor dining and seating.





Permitted Uses

Within the University Village District, the base zone shall dictate the permitted uses for each individual property unless specifically prohibited in the *Prohibited Uses* Section below. In cases where the permitted uses set forth in the Specific Plan for this District are inconsistent with the Zoning Code, the standards of the Specific Plan shall prevail. Base zone permitted uses are found in the Permitted Uses Table, Section 19.150.020(A) of the Zoning Code. All permitted uses are subject to any necessary permits as listed below or in the Zoning Code as well as all other requirements of the Specific Plan and Zoning Code, as applicable. Incidental uses shall be subject to the Incidental Uses Table found in Section 19.150.020(B) of the Zoning Code.

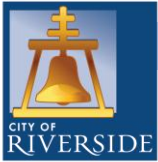
Rezoning for Mixed-Use

As discussed earlier in this chapter, the General Plan Land Use designation of the majority of the properties within the Specific Plan area were change during the General Plan 2025 Program update to Mixed-Use – Neighborhood (MU-N), Mixed-Use – Village (MU-V), or Mixed-Use – Urban (MU-U) (See Figure 7). Within the University Village District, the land use of the properties located north of University Avenue along with the five smaller lots located near the intersection of University Avenue and Iowa Avenue in the southern portion of this District were designated MU-U. While the current base zone of each individual property within the Specific Plan area dictates the standards for land use and development, it is strongly recommended that properties be rezoned to the corresponding mixed-use zone per the Zoning Code and in accordance with their respective General Plan Land Use designations, as they are developed. To encourage mixed use development on these properties, it is recommended that the City initiate the necessary zone changes at the time that a viable project is submitted to the Planning Division for design review.

The land use designations of the multi-family residential development located along Iowa Avenue was designated High Density Residential (HDR). To encourage mixed-use development on this property and to keep consistency with the land use designation of the surrounding properties in this District, it is recommended that the land use designation and zoning be changed to MU-U to encourage mixed-use development on these properties in the future. For properties that adopted the respective mixed-use zone in accordance with their General Plan Land Use Designation, the permitted uses found under the mixed-use zones in the Permitted Uses Table (Section 19.150.020(A)) of the Zoning Code shall apply. Finally, the land use designation of the properties containing the buildings owned and operated by the University of California, including the UCR Extension Center, were designated Public Facilities (PF). While located within the Specific Plan area, these properties are under jurisdiction of the State of California and, therefore, not subject to the development standards of this Specific Plan or the City of Riverside Zoning Code.

Prohibited Uses

Within the University Village District, the base zone shall dictate the prohibited uses for each individual property in addition to the uses listed below. In cases where the prohibited uses set forth in the Specific Plan are inconsistent with the Zoning Code, the standards of the Specific Plan shall prevail. Base zone prohibited uses are found in the Permitted Uses Table, Section 19.150.020(A) of the Zoning Code. Any use which is prohibited by state and/or federal law is also strictly prohibited. The uses listed in *Table 6.8A – University Village District Prohibited Uses* below are strictly prohibited within the University Village District.



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Table 6.8A – University Village District Prohibited Uses

Alcohol Sales – Off-sale ¹
Bail Bonds Offices
Boardinghouse
Bus Terminal
Group Housing (Convalescent Homes, SRO's, Alcohol & Drug Treatment Facilities, more than 6 Occupants)
Home Improvement, Sales and Service – Retail: 20,000 square feet or More
Homeless Shelters
Pawn Shops
Recycling Facilities
Vehicle Fuel Stations
Vehicle Parts and Accessories
Vehicle Repair Facilities
Vehicle Sales, Rental and Leasing – New and Used
Vehicle Wash Facilities

¹ Except as an incidental use to florist shops and establishments that do not propose to sell alcohol as their principal business and contain 15,000 square feet or more of gross floor area. Subject to the Chapter 19.450, Alcohol Sales, of the Zoning Code.

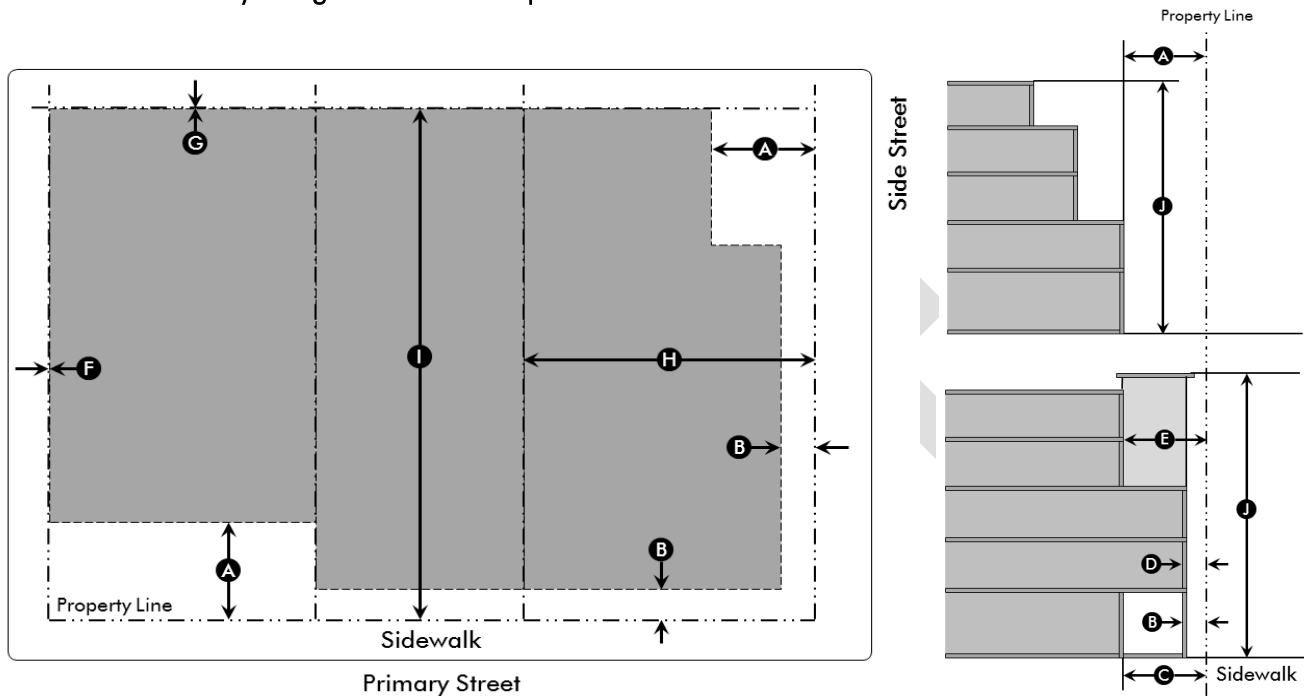
Development Standards

Within the University Village District, the base zone shall dictate the development standards for each individual property with the exception of the standards listed in *Table 6.8B University Village District Development Standards* below. In cases where the development standards set forth in the Specific Plan are inconsistent with the Zoning Code, the standards of the Specific Plan shall prevail.





Table 6.8B University Village District Development Standards



Building Placement		
Setback		
Front/Side Street ^{1,2}	A	15' – 20'
Front/Side Street with Arcade ^{1,2}	B	5' – 10'
Ground Floor	C	15' Minimum
2 nd and 3 rd Floor	D	5' Minimum
4 th Floor and Above ³	E	15' Minimum
Interior Side ⁴	F	0' Minimum
Rear ⁴	G	0' Minimum
Minimum Lot Standards		
Lot Area		Base Zone
Lot Width	H	Base Zone
Lot Depth	I	Base Zone
Maximum Building Height		
Maximum Building Height	J	60'
Front/Side Street Setback Area Standards ^{1,5}		
Surface Coverage ⁶		
Hardscaping		50% – 85%
Landscaping		15% – 50%
Sidewalk Widening ⁷		3' – 9'

Additional Standards for Residential as Part of Mixed-Use Development

Maximum Density	
With less than one acre lot size	20 du/acre
With one to two acres min. lot size	40 du/acre
With more than two acres min. lot size	60 du/acre

Open Space⁸

C. Minimum Private Open Space ⁹	50 sq.ft./du for at least 50% of the units
D. Minimum Common Open Space ¹⁰	50 sq.ft./du

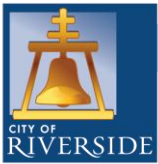
Parking

Mixed-Use

- Parking standards for mixed-use developments shall comply with the Zoning Code, Chapter 19.120.060.B. A reduction in the number of required parking spaces may be permitted for mixed-use developments and/or stand-alone uses in mixed-use zones subject to the approval of a shared parking arrangement.
- See Section 6.4 (General Mixed-Use Development and Design Standards) of this Chapter for additional parking standards.

Base Zone

Parking standards shall comply with the Zoning Code, Chapter 19.580.



Notes

¹See the *Additional Development and Design Standards for the University Village District* Section for additional requirements.

²Applies to building footprint only. Floors can be stepped back to allow for balconies, building articulation and other design features.

³Maximum of 20% of frontage is allowed a minimum 5-foot setback.

⁴15-foot minimum setback required when abutting a residential use.

⁵Applies to setbacks along University Avenue and Iowa Avenue only.

⁶The entire front/side street setback area, including driveways, shall include a mix of decorative hardscaping and dense landscaping.

⁷The existing sidewalk shall be widened by an additional 3 to 9 feet with decorative hardscaping along the entire street frontage.

⁸Can be a combination of indoor and outdoor open space for mixed-use developments. Includes patios and balconies.

⁹Private usable space shall have a minimum dimension on any side of 5-feet. Private usable open space can also be met through equivalent design features as approved by the Planning Commission.

¹⁰Common usable open space may be divided into more than one area; however, each area shall be a minimum of 625-square feet, with no dimension on any side of less than 25-feet.

Additional Development and Design Standards for the University Village District

The following development and design standards apply to all properties within the University Village District.

Front Setback Area (See Figure 21 in Section 6.7)

1. Throughout the University Village District, the pedestrian path of travel (e.g. sidewalks) along the street frontage shall be widened by 3 feet to 9 feet (in accordance with the development standards above) through the use of decorative concrete pavers and/or other decorative hard-surface materials to accommodate higher levels of pedestrian foot traffic.
2. In accordance with the developments standards above, a combination of decorative hardscaping and pockets of dense landscaping shall be used along the entire front setback area (including driveways) to create attractive and useable outdoor gathering spaces that encourage indoor activities to relocate to the outside while drawing pedestrian traffic on-site
3. Building setbacks for projects that incorporate an arcade along the street frontage can be reduced to a minimum of 5 feet and a maximum of 10 feet to accommodate attractive and useable outdoor gathering spaces, in accordance with the standards in Table 6.8B above.
4. Hardscaping materials shall include decorative concrete pavers and/or other decorative hard-surface materials that complement the building(s) on site.
5. Landscaping materials shall include dense, yet drought-tolerant, vegetation arranged in pockets, strategically placed to maximize the usable area within the front setback.
6. Landscaping materials shall include large specimens that provide shade and, at the same time, maintain a clear view of building signs from the street frontage.
7. A combination of raised and ground planters as well as decorative pot containers shall be used for all landscaping areas.
8. Tree grates shall be provided for all trees within the sidewalk adjacent to the front setback area to maximize the pedestrian path of travel.
9. Within the front setback area, a minimum of two outdoor amenities listed in Table 6.8C shall be provided:

**Table 6.8C – Front Setback Outdoor Amenities**

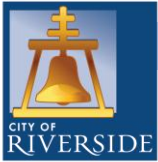
Outdoor Dining	Shade Structures
Water Features	Artistic Bicycle Racks
Art Display Areas	Pave Art
Outdoor Seating	Other Amenities (As deemed acceptable by the Approving Authority)

Building Entrances and Orientation

10. Buildings shall consist of building materials, colors and detailing to fit with the character of the existing surrounding neighborhood.
11. Primary building entrances should be easy to identify along University Avenue and Iowa Avenue.
12. Buildings should be oriented to a defined pedestrian walkway or street.
13. Building materials and colors should exhibit quality and help establish a human scale while providing visual interest.
14. To facilitate the creation of a sense of place, developers are encouraged to incorporate artists into the design team from the inception of planning in order to integrate art into their projects.
15. Special attention should be given to the design of project and building corners as an opportunity to create visual interest and invite activity.
16. Larger sites should feature large plazas and other significant gathering spaces where people can congregate. Public spaces should contribute to the overall sense of place and site identity and help to attract pedestrian users to the development.
17. Loading docks should be located in areas that have the least amount of impact on residential uses.

Building Elevations

18. Ground floor facades of buildings incorporating retail uses should be differentiated from upper stories, allowing for flexibility and change of retail tenants. Retail features, which make up the ground floor façade shall include: retail storefronts; awnings and canopies; sign bands; and cornices. Retail storefronts should be at least 75% glazing and the glazing should be non-tinted and unreflective.
19. Larger buildings with multiple stories should incorporate step-backs into the design of the building elevation(s) facing the street, in accordance with the step-back standards in Table 6.8B above.
20. New infill should be planned and designed in a manner that protects, sustains, and enhances the existing architectural character of University Avenue.
21. In addition to the setback requirements established, the new infill should match the existing building's width, height and massing.
22. New infill buildings should maintain the size and rhythm of space between buildings along the established streetscape.



23. New mixed-use infill development should conform to the proportion, scale and massing of existing residential or commercial buildings adjacent to the infill development.
24. Larger architectural structures can achieve visual harmony with smaller buildings by the use of repetitive massing, roof forms, materials used and cladding color. New, larger buildings may also be subdivided into smaller elements reflective of the mass or detailing of surrounding buildings (i.e. roof form may employ gables, mansard windows, front porches, colonnades, etc.)
25. The entrances to mixed-use buildings should be always oriented to and be highly visible from the street. Entrances for retail uses must be separated from residential entrances. A well-defined separation between different uses in a mixed-use commercial building is important in order to maintain visual clarity. Additional residential or service entrances connected to the rear parking may be provided from the rear or side of the buildings.
26. Materials and textures applied to the front façade should continue around the building.
27. New buildings should complement the basic façade elements, which include a transparent glazed lower storefront, a sign fascia over a projecting cornice and a solid upper façade punctuated by vertical windows.

Urban Open Spaces – Non Residential Development

28. All new non-residential development on lots of greater than twenty thousand (20,000) square feet must provide urban open space. 1 sq. ft./100 sq. ft. gross floor area or 1 sq. ft./200 sq. ft. lot area, whichever is greater shall be provided as public open space. Such open space shall be required to be:
 - a. Accessible and open to the public.
 - b. Improved with seating, plantings, and/or other amenities.
 - c. Visible and accessible from the street or public pedestrian areas.
 - d. Located on the ground floor or no more than five (5) feet above or five (5) feet below ground level.
 - e. Out of doors, or in the open air (may be under a roof or canopy).
 - f. All required open space shall be located behind the sidewalk and on private property.
 - g. For developments on lots in excess of ten (10) acres (four hundred thirty-five thousand, six hundred (435,600) square feet), at least fifty (50) percent of the required open space must be centrally located on the site in a common area.

Base of High Rise Building

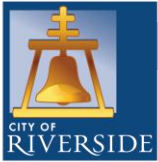
29. For buildings exceeding five (5) stories in height, the following standards shall apply:



- a. The first floor above street grade shall be distinguished from the remainder of the building with an emphasis on providing design elements that will enhance the pedestrian environment. Such elements as cornices, corbelling, molding, stringcourses, ornamentation, changes in material or color, recessing, architectural lighting and other sculpturing of the base as are appropriate shall be provided to add special interest to the base.
- b. In the design of the building facade, attention shall be paid to the appearance both during the day and at night. Material and color changes alone do not meet the requirements of this section and design elements, which are used to meet the requirements of this section, shall be visually continuous around the building. In the event that a building facade is not visible from a public street or right-of-way, then the City Planner, Community Development Director, or their designee (Approving Authority) shall have the option of waiving this requirement.
- c. Special attention shall be given to the design of windows in the base. Band windows are prohibited. Recessed windows that are distinguished from the shaft of the building through the use of arches, pediments, mullions, and other treatments are permitted.

Building Walls Facing a Street

30. All retail and office buildings fronting directly on a public or private street or fronting on a public multi-use path along a transit line and identified in an area plan shall be designed so that the first floor facade of the building(s) along all streets and pathways includes clear glass windows and doors to increase pedestrian interest. These openings shall be arranged so that the uses are visible from and to the street and/or pathway on at least fifty (50) percent of the length of the first floor street level frontage.
31. For all other uses, buildings shall be designed so that the first floor street facade along all streets includes the use of clear glass windows and doors arranged so that the uses are visible from and/or accessible to the street on at least twenty-five (25) percent of the length of the first floor street frontage. When this approach is not feasible, a combination of design elements shall be used on the building facade, or included into the site design, to animate and enliven the streetscape. These design elements may include but are not limited to the following: ornamentation; molding; strong courses; changes in material or color; architectural lighting; works of art; fountains and pools; street furniture; stoops, landscaping and garden areas; and display areas.
32. The first floor facade of all buildings, including structured parking facilities, shall be designed to encourage and complement pedestrian-scale, interest, and activity.
33. Expanses of blank wall shall not exceed twenty (20) continuous feet in length. A blank wall is a facade that does not add to the character of the streetscape and does not contain clear glass windows or doors or sufficient ornamentation, decoration or articulation.
34. No reflective surfaces shall be permitted on street level exterior facades.



35. Ventilation grates on the building, or emergency exit doors located on the first floor street facade(s) shall be decorative and part of the overall building design.

Structured Parking Facilities

36. Structured parking facilities shall be designed to encourage and complement pedestrian-scale interest and activity, and shall be designed so that motorized vehicles parked on all levels of the facility inside are screened from the street, the transitway, and/or from adjacent residentially zoned and/or used property. Decorative elements such as grillwork or louvers may be utilized to accomplish this objective.
37. Openings at the street level are limited to vehicular entrances, pedestrian access to the structure, and ventilation openings. All such openings shall be decorative and be an integral part of the overall building design.

Canopies

38. Canopies, awnings, cornices and similar architectural accents are permitted on exterior building walls. Such features shall be constructed of rigid or flexible material designed to complement the streetscape of the area. Any such feature may extend from the building up to one-half ($\frac{1}{2}$) of the width of the setback area in front of the building or nine (9) feet, whichever is less, and may not be closer than two (2) feet to the back of the curb. In no instance shall such features extend over, or interfere with the growth or maintenance of any required tree plantings. Minimum overhead clearance shall be eight (8) feet. Ground supports for these features are not permitted in the minimum setback, sidewalk or in the public right-of-way. If a canopy, awning, cornice, or other appurtenance extends into the public right-of-way, an encroachment agreement from the City of Riverside Public Works Department shall be required.



7

Chapter 7: Streetscape Design

7.1 Streetscape Setting

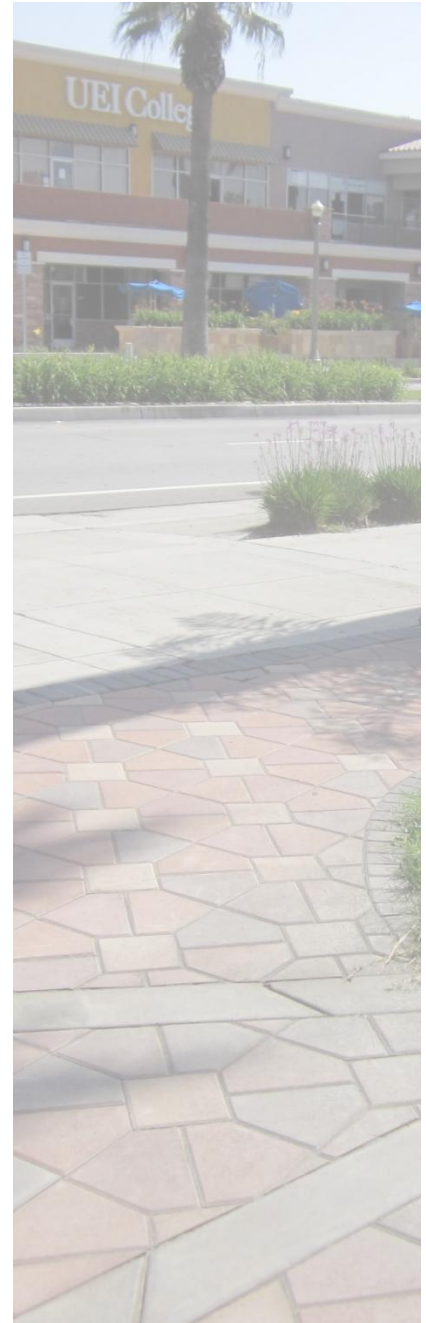
University Avenue has seen many transformations since the early development of Riverside in the late 1800s. First established as Eighth Street, University Avenue historically served as a residential street as a result of the then booming citrus industry and eventually served as a primary link to the University of California Citrus Experiment Station. Residential uses dominated along University Avenue until the early 1950s, when the Avenue was established as a segment of California State Route 60, bringing increased traffic and a need for auto-related support services. The establishment of UCR as a stand-alone university in 1954 and the opening of the Riverside Raceway in what is now Moreno Valley further increased traffic and influenced development along the Avenue. The realignment of State Route 60 in the 1960s, however, led to the financial decline of the businesses along University Avenue well into the 1980s.

University Avenue is now in the midst of a reinvention following several years of property acquisition through the Redevelopment Agency as well as streetscape improvements and landscaping enhancements as part of the recent Riverside Renaissance effort. As a result of Redevelopment efforts, there are many vacant properties along University Avenue that are ripe for development, while the streetscape improvements have established the desired visual palette for future streetscape improvements.

The most recent streetscape improvements along University Avenue include the installation of landscaped center medians between Park Avenue and Mesa Street (west of Chicago Avenue), parkway landscaping, new mature palm trees in front of the Cesar Chavez Community Center/Bobby Bonds Sports Complex, reinforced bus stop paving, and in some locations, enhanced street and sidewalk paving.

Much of University Avenue benefits from having landscaped parkways between the sidewalk and the street, providing a buffer between the vehicular traffic and pedestrians while providing enhanced aesthetics via landscaping. Some landscaped parkways have been eliminated over the years as necessary to accommodate turn pockets at intersections and several bus turnouts.

Palm trees are the most abundant and consistent variety of street trees along University Avenue with California Fan Palm Trees and Chinese





UNIVERSITY AVENUE SPECIFIC PLAN

Flame Trees providing variety. Historically, grass has been used as a ground cover in the parkways and landscape setback areas, however there has been a recent shift towards more native vegetation and water efficient landscaping for recent projects.

7.2 Streetscape Concept

The recommended streetscape concept for University Avenue is to maintain much of the existing mature landscaping and recent streetscape improvements and continue to build upon the established streetscape palette with an increased emphasis on the pedestrian and bicycle environments. To make the University Avenue corridor more pleasant, safe, and inviting for pedestrians and bicyclists, it is important to continue to enhance the streetscape with distinctive street furnishings, lighting, and paving as well as enhanced semi-private gathering spaces.

The development standards of Chapter 6 begin to establish the University Avenue Corridor as a mixed-use corridor with a focus on enhancing the pedestrian environment through site design such as expanding the pedestrian path of travel, pushing buildings closer to the street, orienting the entrances of buildings toward the public right-of-ways, and providing enhanced amenities for pedestrians. The streetscape concept for University Avenue is designed to augment these standards through enhancements to the public realm.

The overall streetscape concept is designed to meet the following objectives:

- Accommodate all modes of transportation with an emphasis on public transportation and enhanced pedestrian and bicycle networks.
- Maintain the existing transportation level of service while accommodating other modes of transportation within the existing right-of ways.
- Visually unify the corridor through consistent paving surfaces, street furnishings, tree varieties and landscaping.
- Create a gateway to Riverside from the I-215/SR-60 freeway.
- Create gateways to the Specific Plan area at Park Avenue, Chicago Avenue, Iowa Avenue and the I-215/SR60 freeway (see Figure 12).
- Continue to maintain and upgrade the existing landscaped parkways.
- Consolidate driveways, where possible.
- Continue to upgrade and enhance crosswalks with decorative paving.



7.3 Streetscape Improvement Guidelines

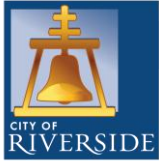
Landscaping

While the recent streetscape and median enhancements have greatly improved the streetscape along University Avenue, the following guidelines should be implemented to preserve the existing mature landscaping and along the corridor and to “bridge the gap” where landscaping is lacking within the public right-of-way:

- The existing Chinese Flame Trees and mature California Fan Palm Trees should be preserved and maintained throughout the Specific Plan area to provide unity and continuity between each District.
- Street trees, shrubs, accent plants and groundcover unique to each District should be established along the existing parkways and medians to reinforce the identity of each District.
- Existing and new canopy trees should be strategically planted and maintained to maximize their shade coverage along the pedestrian path of travel.
- Gateways within the Specific Plan area should accommodate landscaping (including street trees, shrubs, accent plants and groundcover) that is prominent and unique to create identifiable and attractive entrances into the Specific Plan area.

Image 7.3-A: Intersection Treatment





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- Major arterial intersections should accommodate large and unique street trees of the same species to create identifiable nodes. The street trees should be accommodated at the four corners as well as the four medians of the intersection, as depicted in Diagram 7.1:

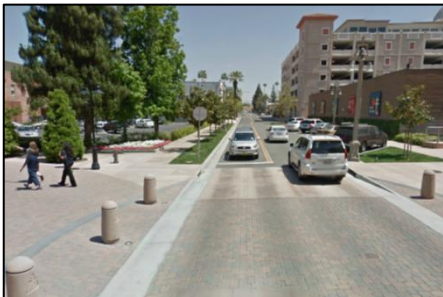
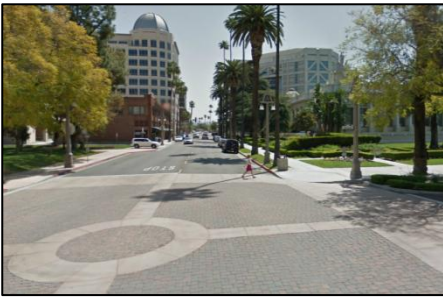
Hardscaping

To make the University Avenue corridor more pleasant, safe, and inviting for pedestrians and bicyclists, it is important to continue to enhance the streetscape with decorative hard surfaces to allow the public and private realms to seamlessly connect. The development standards for front setback areas found in Chapter 6 focus on enhancing the pedestrian environment through site design such as expanding the pedestrian path of travel, pushing buildings closer to the street, orienting the entrances of buildings toward the public right-of-ways, and providing enhanced amenities for pedestrians. As such, the following guidelines should be implemented to augment these standards through enhancements to the public realm:

- Decorative concrete pavers and/or other decorative hard surface materials should be used at all street intersections to improve the visibility of pedestrian crosswalks and create attractive activity nodes (see Image 7.3-A: Intersection Treatment).
- Decorative street bollards should be used at major pedestrian crossings area to minimize traffic speeds and protect pedestrians.
- Where feasible, decorative concrete pavers and/or other decorative hard surface materials should replace the existing concrete sidewalks.
- Where streets trees are planted outside of a landscaped parkway, decorative tree grates should be provided to expand and enhance the pedestrian path of travel.

Lighting

Since, and in accordance with, the 1993 update to the University Avenue Specific Plan, a series of pedestrian-scale, acorn globe street lights were installed along University Avenue, between the I-215 freeway and Ottawa Avenue. However, no other improvements related to street lighting have been completed since this time. To achieve the vision of the transforming the University Avenue corridor into a lively and pedestrian-friendly corridor, it is critical that the following guidelines related to lighting within the public right-of-way be implemented to increase the safety and security of pedestrian along the corridor as well as to augment and complement the development standards for front setback areas found in Chapter 6:



7: STREETScape DESIGN



- The existing pattern of pedestrian-scale, acorn globe street lights should continue to be provided along University Avenue, Iowa Avenue and Chicago Avenue.
- All gateways entrances should include decorative lighting to illuminate architectural features and decorative landscaping to create identifiable and attractive entrances into the Specific Plan area.
- Where feasible, projected and concealed landscape lighting should be provided within all parkways and landscaped medians to accent decorative landscaping during nighttime hours.

Street Furnishings

In addition to the landscape lighting and distinct paving, the appearance of the University Avenue corridor can be improved and District identity reinforced with other distinctive streetscape elements, such as street furnishings. These elements encourage pedestrian activity at appropriate nodes along the corridor, promote safety, and direct pedestrians, drivers and transit riders. Currently, street furniture is primarily limited along University Avenue to bus shelters, which provide benches, trash receptacles and shade for transit riders.

In order to achieve the vision of the transforming the University Avenue corridor into a lively and pedestrian-friendly corridor, it is critical that the following guidelines related to street furnishings be implemented to improve the pedestrian experience along the corridor as well as to augment and complement the development standards for front setback areas found in Chapter 6:

- A consistent palette of street furnishings, including, shade structures, benches, trash receptacles, bicycle racks, and tree grates should be implemented along the entire corridor to allow pedestrians and bicyclists to easily transition between Districts.
- Benches should be placed wherever there is a high volume of pedestrian activity, including at major arterial intersections, to supplement the existing bus shelters.
- Metal benches should be used, given that they are more easily maintained than wood or concrete and are less susceptible to vandalism.
- Trash receptacles and bicycle racks that complement each other in terms of design and materials should be implemented at the same locations.

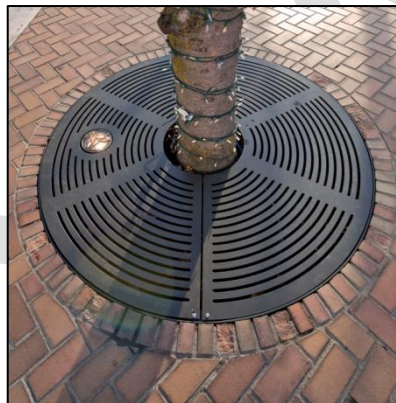


The existing bus shelters were designed in accordance with 1993 update to the University Avenue Specific Plan.



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- Additional trash receptacles should be placed where needed, generally at most intersections as well as mid-block in areas with high levels of pedestrian activity.
- Other design elements such as drinking fountains, water features, art display areas, tree grates, raised and ground planters, decorative pot containers, shade structures and artistic bicycle racks should be considered in conjunction with the front setback area development standards to add to the aesthetic value of the streetscape as well as provide valuable amenities for pedestrians and bicyclists.



Gateway Treatments

Gateways serve to frame the main entry points into the Specific Plan area, create a sense of arrival and contribute toward the overall beautification of the area. As identified on Figure 12 (Local Circulation and Linkages), gateways to the Specific Plan area are located at Park Avenue, Chicago Avenue, Iowa Avenue and the I-215/SR60 freeway. Currently, these gateways provide very little aesthetic appeal and, for the most part, fail to create a sense of arrival. While examples of gateways in other areas (including the Riverside Marketplace) incorporate treatments such as arched gateway signs spanning the street and announcing the name of the area, the treatment of gateways within the University Avenue Specific Plan area should be accomplished in a more subtle manner. As such, the

7: STREETScape DESIGN



following guidelines address the prescribed treatment of gateways to the Specific Plan area:

- Development projects proposed adjacent to the identified gateways should include prominent and unique architectural elements to create a sense of arrival to the University Avenue corridor as well as to the individual Districts (see Figure 4).
- The public right-of-way near gateways, including parkways and medians, should accommodate landscaping (street trees, shrubs, accent plants and groundcover) that is prominent and unique to create identifiable and attractive entrances into the Specific Plan area.
- Gateways entrances should include decorative lighting to illuminate architectural features and decorative landscaping to create identifiable and attractive entrances into the Specific Plan area.
- Decorative concrete pavers and/or other decorative hard surface materials should be accommodated at all street intersections adjacent to gateways to improve the visibility of pedestrian crosswalks and create attractive activity nodes.

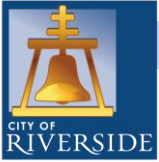
Pedestrian Network

Central to the overall vision of the University Avenue Specific Plan is the concept of implementing an urban design fabric that focuses on the pedestrian scale. The standards found in Chapter 6 are designed to give priority to the front setback area through the creation of attractive and usable outdoor gathering spaces that encourage indoor activities to relocate to the outside while drawing pedestrian traffic on-site. The seamless connection between these gathering spaces and the pedestrian path of travel along the public right-of-way is critical to the functionality and success of these spaces. As such, the pedestrian path of travel within the public right-of-way should be optimized to allow higher levels of foot traffic. In addition to the pedestrian improvements recommended in the prior section of this Chapter, the following additional guidelines address the prescribed improvements to the pedestrian network (see Figure 12):

- Within the Commercial Core District, pedestrian connectivity mid-block across Chicago Avenue should be improved to better connect the Town Square Shopping Center with the Chicago Center Shopping Area through the implementation of textured paving and/or other paving material to clearly identify the pedestrian path of travel across Chicago Avenue.
- Within the University Village District, the connection between the UCR campus and the University Village site is severely hindered by



The connection between the UCR campus and the University Village site is severely hindered by the I-215 Freeway.



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the I-215 Freeway, particularly for pedestrians and bicyclists. To achieve the vision of University Village becoming the next town-and-gown village, it is imperative that pedestrian improvements to the University Avenue underpass at the I-215 Freeway be designed so that the pedestrian experience is greatly improved through enhanced landscaping, lighting and other measures to create a sense of arrival. The Student Entertainment Charrette and Action Plan, adopted in 2010, included several recommendations to improve this vital connection. Figure 22 illustrates these improvements.



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Figure 22
SR-60/I-215 Freeway Underpass



7: STREETScape DESIGN



Alleyways

Alleyways within the Specific Plan area are primarily located within the Eastside Neighborhood District. The majority of properties within this District are characterized by small shallow lots that abut an alleyway along the rear. These alleyways serve to provide access to the properties fronting University Avenue as well as the adjacent residential properties. The alleyways are also used by the residents in the area and are, in fact, a part of the pedestrian network within this District. However, there are various challenges with the infrastructure of the alleyways that hamper their accessibility. Many of the alleyways are too narrow to accommodate utility and delivery vehicles. Other alleyways remain unpaved and are susceptible to weeds and debris. Lighting is another challenge that diminishes the sense of security for users of the alleyways. Finally, because of poor infrastructure, many of the alleyways have become places that facilitate graffiti, vandalism and other nefarious activities. To improve the quality of the alleyways and better connect them to the larger pedestrian network, the following guidelines should be considered:

- All unpaved alleyways should be paved to improve their accessibility and to prevent the growth of weeds and dry brush.
- Pedestrian scale lighting for all alleyways should be implemented to improve visibility during nighttime hours, discourage nefarious activities and contribute to their overall sense of security.
- Unnecessary alleyways should be vacated, provided that they are not used for vehicular and/or pedestrian access.
- The incorporation of artistic murals within the alleyways by local artists should be explored to add aesthetic value to the alleyways and contribute to neighborhood pride.



Artistic murals can add aesthetic value to alleyways as well as contribute to neighborhood pride.



The murals at the University Avenue/I-215 underpass are an example of successful public art.

Public Art

Another design element that can enhance the environment of the corridor is public art, which can take many forms and be introduced in many places. It can easily add value to a district, corridor, and community. It can speak to the history of a place, the story of an individual, or the identity of an object. An example of successful public art within the Specific Plan area is the murals at the University Avenue/I-215 underpass. Given the surrounding neighborhoods and the proximity to UCR and Downtown, there is an abundance of talented local artists that can contribute greatly to public art along the University Avenue corridor. As such, the following guidelines relate to the implementation of public art along within Specific Plan area:

- Public art display areas where local artists can display their art on a rotating basis should be incorporated into the existing bus shelters.



Artist-designed intersection paving reinforces district identity.



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- The identity of each District should be reinforced through the incorporation of “paving murals” at prominent intersections, including Park Avenue, Chicago Avenue and Iowa Avenue that relates to each District (see Image 7.3-A – Intersection Treatment). The mural itself could be artist-designed, and should utilize the same materials in each district.
- Design elements such as drinking fountains, water features, art display areas, tree grates, raised and ground planters, decorative pot containers, shade structures and artistic bicycle racks can all be designed and designated as public art to add to the aesthetic value of the streetscape as well as provide valuable amenities for pedestrians and bicyclists.

Street Cross Sections

Since the 1993 update to the University Avenue Specific Plan, several phases of streetscape improvements have been completed along the University Avenue corridor, including historically inspired street lights, street trees, bus shelters, bicycle lanes and landscaped medians. The most recent streetscape improvements include the installation of landscaped center medians between Park Avenue and Mesa Street (west of Chicago Avenue), parkway landscaping, new mature palm trees in front of the Cesar Chavez Community Center/Bobby Bonds Sports Complex, reinforced bus stop paving, and in some locations, enhanced street and sidewalk paving.

Much of University Avenue benefits from having landscaped parkways between the sidewalk and the street, providing a buffer between the vehicular traffic and pedestrians while providing enhanced aesthetics via landscaping. Some landscaped parkways have been eliminated over the years as necessary to accommodate turn pockets at intersections and several bus turnouts.

With the exception of the recommended improvements found in the prior sections of this Chapter, the streets within the Specific Plan area are mostly fully improved in terms of medians, sidewalks and number of drive lanes. The updates to the General Plan in 1994 and the Specific Plan in 1993 both envisioned University Avenue as a four-lane arterial in order to accommodate a more pedestrian-friendly streetscape. Consequently, University Avenue is currently designated as a four-lane 100-foot arterial on the Circulation Element of the General Plan 2025. The following are descriptions of the existing conditions for the major streets within the University Avenue Specific Plan area:

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University Avenue - West of Victoria Avenue

Curb-to-Curb:

The street cross section generally consists of 70 feet of paving, curb-to-curb, accommodating four travel lanes, a 5-foot bicycle lane in each direction and a 12-foot landscape median consisting of grass, street trees and shrubs.

Landscaped Parkway:

The streetscape provides for 6.5-foot wide curb-adjacent parkways on both sides of University Avenue consisting of hardscaping, grass and street trees.

Sidewalk:

The sidewalk section includes 6 feet of parkway-adjacent paving on both sides of University Avenue.

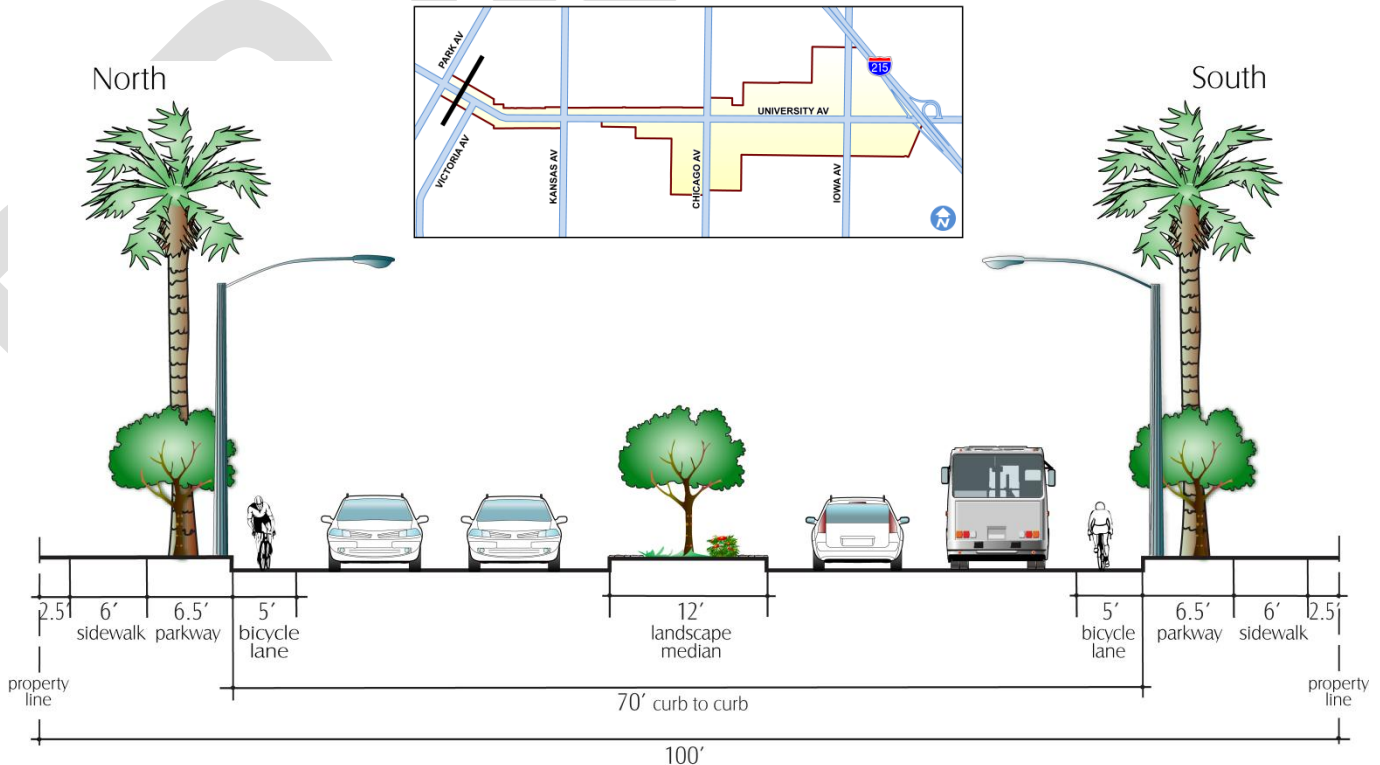
Street Trees:

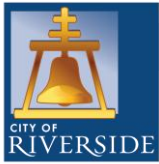
The existing street tree pattern consists of California Fan Palm Trees and Chinese Flame Trees in the parkways and median.

Street Lights:

Street lights consist of 40-foot high standard street lights.

Diagram 7.2: University Avenue – West of Victoria Avenue





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University Avenue – Between Chicago Avenue and Iowa Avenue

Curb-to-Curb:

The street cross section generally consists of 66 feet of paving, curb-to-curb, accommodating four travel lanes, a 5-foot bicycle lane in each direction and a 10-foot turning lane in the center.

Landscaped Parkway:

The streetscape provides for 7.5-foot wide curb-adjacent parkways north of University Avenue and 6.5-foot wide curb-adjacent parkways south of University Avenue consisting of street trees and shrubs.

Sidewalk:

The sidewalk section includes 10 feet of parkway-adjacent paving north of University Avenue and 7 feet of parkway-adjacent paving south of University Avenue.

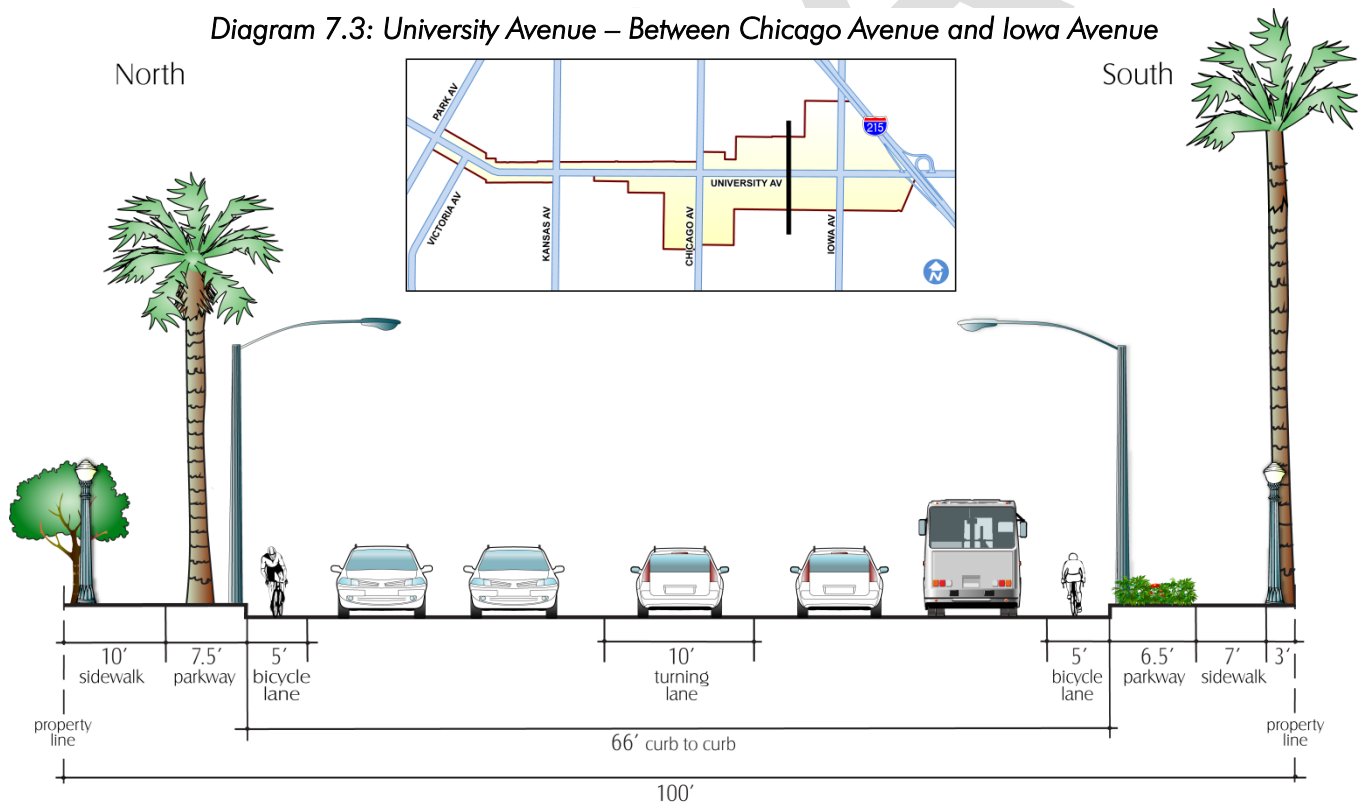
Street Trees:

The existing street tree pattern consists of California Fan Palm Trees and Chinese Flame Trees in the parkways.

Street Lights:

Street lighting consists of 18-foot high acorn globe street lights and 40-foot high standard street lights.

Diagram 7.3: University Avenue – Between Chicago Avenue and Iowa Avenue



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University Avenue – Between Iowa Avenue and I-215 Freeway

Curb-to-Curb:

The street cross section generally consists of 89 feet of paving, curb-to-curb, accommodating four travel lanes, a 5-foot bicycle lane in each direction and a 6 to 16-foot landscape median consisting of hardscaping, street trees and shrubs.

Landscaped Parkway:

The streetscape provides for 9.5-foot wide curb-adjacent parkways south of University Avenue consisting of street trees and shrubs and no parkways north of University Avenue.

Sidewalk:

The sidewalk section includes 11 feet of curb-adjacent paving north of University Avenue and 7.5 feet of parkway-adjacent paving south of University Avenue.

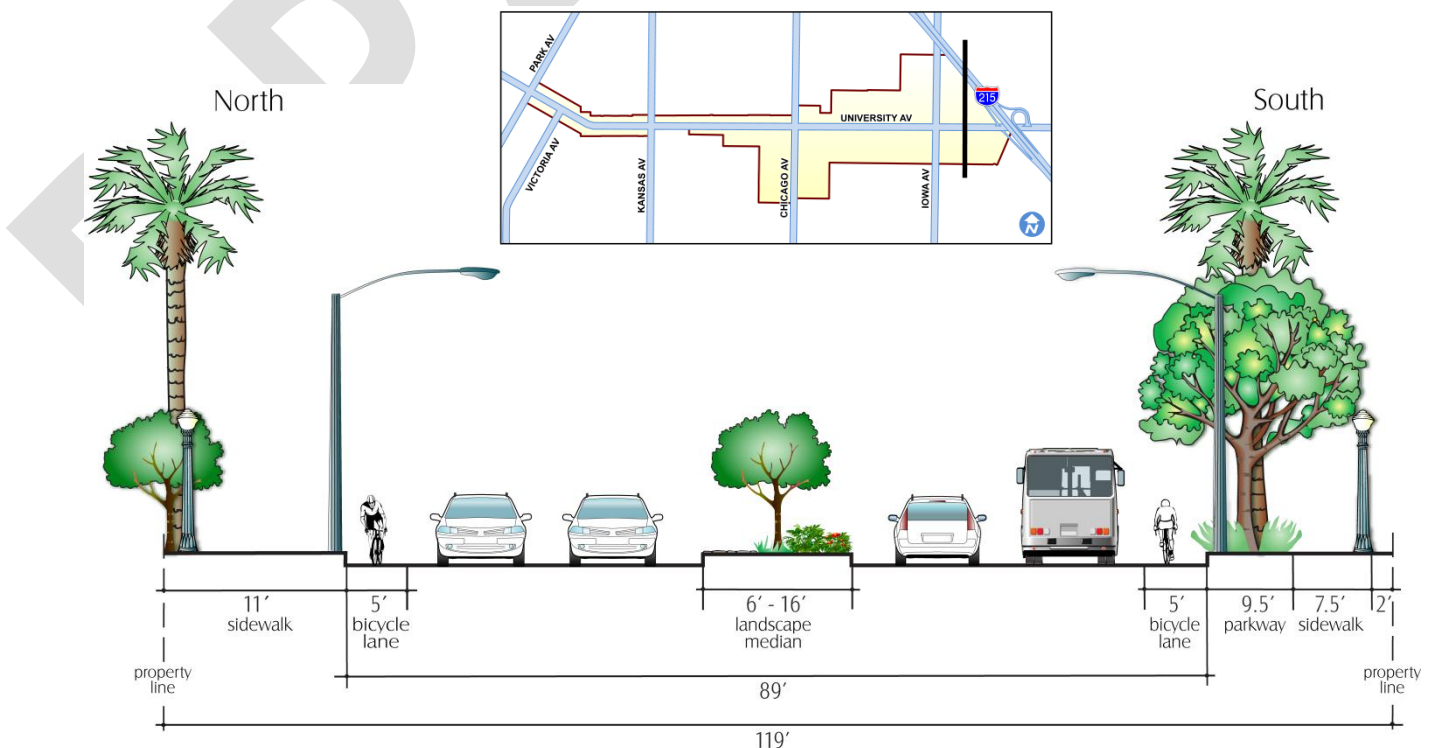
Street Trees:

The existing street tree pattern consists of California Fan Palm Trees and Chinese Flame Trees in the parkways.

Street Lights:

Street lighting consists of 18-foot high acorn globe street lights and 40-foot high standard street lights.

Diagram 7.4: University Avenue – Between Iowa Avenue and I-215 Freeway





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SECTION III: IMPLEMENTATION



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8

Chapter 8: Implementation

8.1 Specific Plans and Private Economic Investment

Specific Plans generally provide direction for future development and improvements for both the public and private sectors. The ultimate goal of such a planning effort is to attract desired private economic investment to a specific area. The University Avenue Specific Plan sets forth a common vision, policies and standards for private development as well as recommends various public improvements.

Cities can facilitate private development by creating an environment conducive to development through the actions and policies of a Specific Plan. These include, but are not limited to:

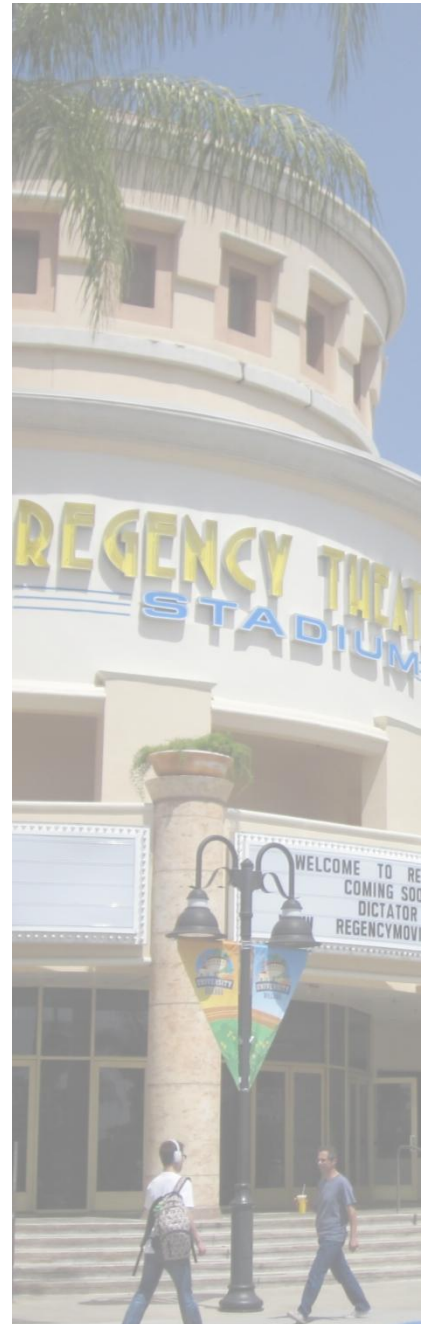
- Creating zoning that is responsive to the market demand for various land uses;
- Allowing increased density in appropriate areas;
- Allowing parking reductions in higher density areas with transit and shared parking opportunities;
- Streamlining the entitlements process for desired project types;
- Providing area-wide public improvements, such as parking, infrastructure, landscaping, street furniture, etc.;
- Marketing the area with a new or enhanced identity; and
- Providing code enforcement to improve the visual appeal of the area.

The Specific Plan accomplishes all of these items through adoption of the Plan itself, as well as through future implementation of the Plan by various City departments.

In addition to the above actions and policies, cities can also provide direct or indirect financial assistance to area businesses, property owners, and key development projects. This could include initiatives such as waiving or reducing various local fees and taxes, assembling development sites, providing loans and/or grants for various business and property improvement purposes such as building facade improvements and investing in site- or project-specific infrastructure.

8.2 Mixed-Use Development Potential

The University Avenue Specific Plan acts as an overlay zone to the base zoning established in the Riverside Zoning Code and Zoning Map. With the exception of the rezoning of the Town Square Shopping Center site in





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the Commercial Core District in conformance with the Housing Element, adoption of the Specific Plan will not automatically rezone properties with the Specific Plan area.

However, to conform to the General Plan 2025 as well as achieve the Vision of the Specific Plan, it is strongly recommended that properties be rezoned to the corresponding mixed-use zone in accordance with their respective General Plan Land Use designations, as they are developed. To encourage mixed use development on these properties, it is recommended that the City initiate the necessary zone changes at the time that a viable project is submitted to the Planning Division for design review.

The importance of land use changes within the Specific Plan lie in the properties that have been designated mixed-use in General Plan 2025, but have not yet been rezoned to mixed-use. The General Plan Land Use designation of the majority of the properties within the Specific Plan area were changed during the General Plan 2025 Program update to Mixed-Use – Neighborhood (MU-N), Mixed-Use – Village (MU-V), or Mixed-Use – Urban (MU-U) (see Figures 7). These properties, due to their location, urban quality, development potential, and proximity to transit, offer a prime opportunity for higher density residential and mixed use projects that will be required for financially viable development. By adopting the mixed-use zoning recommended in the Specific Plan, a greater variety and mixture of land uses and structures will be permitted than what is allowed by the current base zone, including a higher density for residential projects. Implementing catalytic projects at opportunity sites will help to “kick start” revitalization along the entire corridor.

In addition to land use changes, the implementation strategy should focus on streetscape improvements, particularly landscaping, hardscaping and urban amenities in the public realm discussed in Chapter 7 – Streetscape Design. This can be accomplished through both City-financed improvement projects and developer dedications and improvements. The Specific Plan should also focus on revitalization of existing development, including adaptive reuse of existing structures. The flexibility in site planning offered by the mixed-use zones will help accomplish this goal.

8.3 Recommended Improvement Projects

The University Avenue Specific Plan includes a number of tools designed to assure the development and maintenance of the overall area and its districts in accordance with the Plan’s vision. These tools include lists of permitted land uses, development standards, design guidelines and streetscape improvement recommendations. These tools are largely intended to be employed as development occurs and, as such, they are more “passive” in nature. Beyond these more passive tools, the full implementation of the University Avenue Specific Plan will need a number of proactively initiated public and semi-public improvements and programs to help “jumpstart” and organize projects that would not likely

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occur in the normal course of private development. These implementation programs/projects are described below.

Utilities Lines

Improve utilities as follows: To the extent feasible, place all utility lines within the Specific Plan area underground to improve aesthetics. While there are no aboveground utility lines along the University Avenue corridor, aboveground lines cross University Avenue at Park Avenue, Eucalyptus Avenue, Kansas Avenue, Ottawa Avenue, Cranford Avenue and Iowa Avenue. Screen all utility equipment properly and place outside of the pedestrian path of travel.

Streetscape Improvement

Implement streetscape improvements throughout the Specific Plan area related to landscaping, hardscaping, lighting, street furnishings, gateway treatment, pedestrian network, alleyways and public art in accordance with the guidelines found in Chapter 7: Streetscape Design.

Freeway Underpasses

Work with Caltrans to ensure that the University Avenue underpass at the I-215 Freeway includes lighting, landscaping and hardscaping to complement the existing murals and enhance the pedestrian environment.

Transit

Improve transit as follows: Support and aid the Riverside Transit Agency (RTA) as needed to facilitate implementation of the proposed Bus Rapid Transit (BRT) along the “L” corridor. Establish a shuttle bus system linking UCR, Marketplace, and Downtown. Consider using an arts-oriented concept, such as an “A.R.T.S. Bus” as used in Old Pasadena, to highlight the cultural, historic and art features of Riverside.

Coordination with Marketplace and Downtown

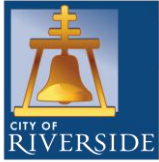
Strengthen the connection to the Marketplace and Downtown through the coordination of street improvements, promotions, events, roadway improvements, transit, and the like.

Truck Traffic

Discourage or divert industrial truck traffic on University Avenue by methods as determined appropriate by the Planning Division and Public Works Department.

8.4 Recommended Implementation Strategies

This section includes strategies that the City should initiate to implement the objectives and recommendations presented in the University Avenue Specific Plan.



UNIVERSITY AVENUE SPECIFIC PLAN

Implement Mixed-Use Zoning

Implement mixed-use zoning in the areas designated Mixed-Use in General Plan 2025 to provide clear direction and regulations for new development. To incentivize mixed-use development, it is recommended that the City initiate the required zone changes, per Article V of the Municipal Code, to the appropriate mixed-use zone (consistent with the General Plan 2025), concurrent with applicant submittals for Site Plan Review.

Prepare a Comprehensive Streetscape Plan

Prepare a Comprehensive Streetscape Plan for University Avenue based on the guidelines and recommendations found in Chapter 7 – Streetscape Design. The Streetscape Plan should identify exact street tree locations, establish minimum tree sizes, and set forth a plan for removal/relocation of existing trees that are not part of the landscape concept. The Streetscape Plan should also address pedestrian-scaled lighting, street furniture, sidewalks and intersection improvements.

A Streetscape Plan is necessary in order to provide information to developers of large projects about where and how to install public improvements along their property frontage. It is also important for the City to continue to implement public improvements along the University Avenue corridor where the private sector is not expected to make the improvements. A Streetscape Plan is crucial component of Specific Plan implementation in order to view University Avenue as a cohesive, unified corridor. The recommended improvements of the Streetscape Plan should be included in the City's Capital Improvement Program (CIP).

Assist in Land Assembly

Assist private developers in land assembly and lot consolidation. In the past, development in a largely built-out Specific Plan area was facilitated by land assembly efforts of the public sector through Redevelopment Agencies, where a city purchases small parcels of land and assembles them into market-ready development sites that are resold to private developers. With the abolishment of Redevelopment Agencies by the State of California, other mechanisms for land assembly should be explored by the City and made available to developers.

Increase Joint Planning Efforts with the Riverside Transit Agency (RTA)

Increase joint planning efforts between the City and the RTA to ensure that Bus Rapid Transit (BRT) is implemented along the "L" Corridor (see Figure 13) and to coordinate the location of BRT stations with supporting transit-oriented development projects and major pedestrian nodes. Keep RTA informed of new projects submittals in the Specific Plan area and allow for RTA review of larger projects for transit-friendly design elements early in the development process.

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Incentivize Development

Incentivize the development directions of the Specific Plan. In addition to the University Avenue Green Developer Program (see Chapter 5), the City can create incentives for development of the corridor through public subsidies, public loans, the expediting of the City review processes or improving publicly-owned properties.

Consider Parking Districts

Consider Parking Districts at appropriate locations along the corridor to address future parking needs and encourage development in the Specific Plan area. There are a variety of ways to establish and implement Parking Districts, however, the process generally follows these steps:

- Hire a traffic consultant who specializes in parking studies and parking management.
- Work with businesses, property owners and developers to define and target potential sites for parking areas.
- Purchase sites as they become available, and according to the priorities established with businesses, property owners and developers.
- Determine the amount for in-lieu fees based on land costs and estimated program costs. This differs considerably between communities depending on location, market value of land, and varying programs and facilities (i.e., surface parking or structured parking).
- Parallel to the above steps, work with City Attorney to set up the legal procedures for in-lieu fees.

Consider Creating Business Improvement Districts (BIDs)

Consider the establishment of BIDs within each District of the Specific Plan area to fund improvements and maintenance (see Section 8.5 Funding Mechanisms for more information on BIDs).

8.5 Funding Mechanisms

Like the improvements already installed along University Avenue, future Specific Plan improvements will be implemented by through the efforts of both private developers and City involvement. Many public improvements will be installed with the development of adjacent land, while other improvements, not directly associated with adjacent development will need to be installed through public funding. Many other opportunities will involve the efforts of both public and private entities working together toward a common goal. Beyond purely private sources, the public and semi-public forms of funding in this section may be used to help implement the University Avenue Specific Plan.



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Capital Improvement Program

The Capital Improvement Program (CIP) is the City's multi-year planning instrument used to facilitate the timing and financing of capital improvements. The CIP identifies the funds available for capital improvement projects and the priority use of these funds. Planned infrastructure improvements along the University Avenue Corridor could be added to the City's CIP listing.

Community Development Block Grants (CDBG)

Community Development Block Grants are annual grants provided to cities and counties from the U.S. Department of Housing and Urban Development (HUD) for use towards economic development, public facilities, and housing rehabilitation. Communities receiving CDBG funds are eligible to apply for Section 108 loans to fund large development projects. The loans are repaid through a community's future CDBG funding. Loans may be used for property acquisition, rehabilitation of publicly owned real property, related relocation, demolition and site work, financing costs, and housing rehabilitation. Using this mechanism, the City could raise some of the improvement funds for the Specific Plan area on a one-time basis for economic development purposes.

Transportation Funding Sources

There are a variety of county, state and federal funding sources potentially available for transportation infrastructure. Measure A, passed by Riverside County voters in 1988, raised the County sales tax one-half cent for transportation projects that include carpool lane construction, Metrolink commuter rail service, improvements to intersections and congested roads, and specialized transportation services. The State Transportation Improvement Program (STIP)/Regional Improvement Program (RIP) provides funding toward the improvement of transportation systems including state highways, local roads, public transit, pedestrian and bicycle facilities, and intermodal facilities. Federal transportation funds, available through the Transportation Equity Act for the 21st Century (TEA-21), provides an annual allocation of Federal funds for highways and surface transportation, mass transit, and highway safety. Programs funded under TEA-21 include a transit enhancement program to improve the quality of life in or around transportation facilities. Transit enhancement activities include landscaping and scenic beautification, historic preservation, acquisition of scenic easement and scenic or historic sites, provision of pedestrian and bike facilities, and control and removal of outdoor advertising. Although it is recognized that transportation funding is extremely competitive and available funds are limited, the adoption of a Specific Plan with a strong transit component can improve the City's ability to obtain funding.

Business Improvement Districts

A Business Improvement District (BID) is a defined geographical area in which business owners and/or property owners agree to assess themselves

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annual fees as a means of funding activities and programs intended to enhance the business environment. The fees can be applied toward a wide range of activities including, but not limited to, marketing and promotion, security, streetscape improvements and maintenance, parking improvements, public art, and special events. Once established, the annual BID fees are mandatory for businesses/properties located within the BID boundary. By pooling resources and coordinating efforts, the business community can invest in mutually beneficial improvements that they could not afford or manage individually.

Special Assessment Districts

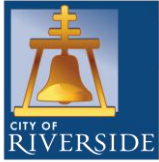
Special Assessment Districts are used to fund capital facilities such as roads, water, sewer, and flood control, as well as streetscape improvements, landscape and other use-related improvements within a defined district. This could be especially beneficial along University Avenue. Special Assessment Districts are created to shift the financing of infrastructure from all taxpayers to only those who benefit specifically from the improvement. Typically, property owners petition a city to form a district to finance large-scale infrastructure improvements. Special Assessment Districts require assessments on property owners in proportion to benefit received.

Parking Districts/In Lieu Fee Program

Parking Districts provide funding for the acquisition, improvement, and operation of shared parking facilities. Funds are collected through an assessment on properties located within the district that will benefit from the parking. Parking Districts can be very effective tools to help create more parking and efficient use of existing parking spaces. Formation of one or more Parking Districts at key nodes along the corridor will allow owners of smaller buildings to avoid providing their own on-site parking and ensure that parking in the area is managed and operated in an efficient manner. Shared parking arrangements help to maximize the use of parking resources, both public and private, in the near-term and future.

City assistance toward the cost of parking provides an important incentive to new business (retail, restaurant) development. In other words, this removes the financial burden of parking from the developers or operators of retail/restaurant space, making their investments more competitive. The City investment in parking can be somewhat offset by user fees.

Payment of in-lieu parking fees by private developers can also contribute toward the provision of public parking. Developers can be relieved of their on-site parking obligations through payment of a parking in-lieu fee. On this basis, then, the City would have in place a mechanism to recapture some portion of its investment in parking.



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Private Property Owners/Developer Exactions

Many cities require that private developers contribute funds toward, or directly install, selected public improvements such as circulation and streetscape improvements. This option may be appropriate for certain types and scale of development along University Avenue, particularly where the private development will directly benefit from the proposed public improvements.

Historic Investment Tax Credits

There are several historic buildings and buildings eligible for historic designation along University Avenue within the Specific Plan area. Federal investment tax credits for historic rehabilitation are available to all income-producing properties that are listed in, or eligible for listing in, the National Register of historic places either individually or as a contributor to a district. A project can qualify to recover 20% of its rehabilitation costs. Buildings that are not listed in, or eligible for, the National Register, but were put in use prior to 1936 can qualify for a tax deduction, which is 10% of the rehabilitation costs.

Low Income Housing Tax Credits

Available to developers of affordable rental housing, the Low Income Housing Tax Credit (LIHTC) program allows developers of affordable housing projects to receive a tax credit against an investor's income tax liability. At least 20% of a project's units must be occupied by and affordable to households with incomes at 50% of Area Median Income (AMI); or at least 40% of the project's units must be occupied by households at 60% of AMI.

Tax-Exempt Multi-Family Housing Bonds

Tax-exempt multi-family housing bonds provide below-market financing for affordable rental projects. The bonds are allocated by the California Debt Limit Allocation Committee (CDLAC). For interest on bonds to be tax-exempt, the bonds must be issued by a city, county, housing authority, or redevelopment agency. Proceeds from the sale of the bonds can be used for new construction and permanent financing as well as the purchase and rehabilitation of an existing property.

Home Investment Partnership Program

The Home Investment Partnership Program (HOME) provides Federal funding to state and local jurisdictions for the purpose of expanding the supply of affordable housing for very low and low-income households and building the capacity of local nonprofit developers and state and local governments. Jurisdictions receiving HOME funds are required to provide matching state, local, or private funds at a ratio of one dollar for every four HOME dollars expended. HOME funds can be used toward tenant-based rental assistance, relocation costs for persons displaced by HOME activities, rehabilitation of existing properties, the new construction of rental or for sale housing, and financing assistance for first-time

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homebuyers. HOME funds are allocated based on a formula reflecting a jurisdiction's housing need. Participating jurisdictions must set aside 15% of their allocations for housing owned, developed, or sponsored by Community Housing Development Organizations (CHDOs).

8.6 Maintenance Responsibilities

Maintenance responsibilities within University Avenue Specific Plan area are divided between the City of Riverside, individual property owner(s), and utility companies. Public streets within the project area are owned and maintained by the City of Riverside.

Water, Electric, and Streetlight Utilities

City Public Utilities Department owns and maintains all water lines, electric lines, and streetlights in public rights-of-way and public easements. The City is not responsible for the maintenance of any individual service lines to private development or private lighting.

Water and Sewage Drainage Facilities

Water, sewage, and drainage improvements located within public rights of way and easements are and maintained by the City of Riverside Public Works Department. The City is not responsible for the maintenance of any individual service lines to private development.

Other Utilities

The utility companies associated with the respective services are responsible for the maintenance of the natural gas and communication services throughout the Specific Plan area.

Parks

The Cesar Chavez Community Center and Bobby Bonds Sports Complex are maintained by the City of Riverside Park and Recreation Department.

8.7 Recommended Phasing Strategy

The purpose of the phasing strategy is to describe the general approach to achieving the most revitalization within the University Avenue Specific Plan area in the least amount of time. The phasing strategy for revitalization requires a public/private partnership with coordinated investments.

Step 1: Implement Mixed Use Zoning

Begin working with developers to implement mixed use zoning on catalytic sites.

Step 2: Prepare a Comprehensive Streetscape Plan

Prepare a Streetscape Plan for the entire corridor, as described in Section 8.4 of this Chapter. The Streetscape Plan should recommend cohesive, unified improvements for the streetscape based on the guidelines and recommendations in Chapter 7 of this Specific Plan.



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Step 3: Obtain Funding

Apply for available outside funding sources and/or utilize available City funding for all, or portions of, the Streetscape Plan. Applicable outside funding sources include County or State funds, gas tax funds, grants, etc.

Step 4: Create Marketing Materials/Target Developers

Create developer recruitment materials, such as brochures, presentation materials, for the corridor as a whole or for selected areas along the corridor. Conduct a targeted solicitation of desired developers based on research of past projects and types of development, consistent with the permitted uses and design standards of the Specific Plan.

Step 5: Recruit Developers and Tenants

Enter into a developer recruitment process. The City can take an active role in identifying and recruiting desirable tenants. This should be closely coordinated with the developer recruitment.

Step 6: Schedule Implementation

A five-year implementation plan should be prepared that considers the timing of public improvements and public/private partnerships in order to maximize effectiveness.

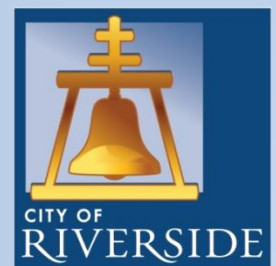
Step 7: Revisit and Revise the Specific Plan

The University Avenue Specific Plan should be periodically revisited to ensure that new development and improvements are meeting the overall vision, goals and objectives. If certain recommendations are not having the anticipated effects, they should be modified as necessary.





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City of Arts & Innovation